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Town Center General Plan Report

Foster City, California

November, 1983



Town Center General Plan Report

Foster City, California

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November, 1983

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Introduction

Overview

This report is the culmination of an extensive study in which a General Plan for the Town Center project has been developed. The process has involved an identification of the environmental, social and aesthetic issues affecting the site and its surroundings; the development of goals and objectives that respond to these issues; the development of a plan that addresses these project goals and objectives; and a set of guidelines that provide the framework in which Town Center will be implemented.

(Handwritten note: This section is a summary of the history of the project and provides a discussion of Town Center as it relates to its regional as well as its local context. It concludes with an identification of the project's goals and objectives accompanied by a discussion of each.)

The report is organized around this process of General Plan development. The Introduction summarizes the history of the project and provides a discussion of Town Center as it relates to its regional as well as its local context. It concludes with an identification of the project's goals and objectives accompanied by a discussion of each.

The second section is the description of the General Plan. Design concepts and a descriptive narrative of the elements of the Plan are accompanied by graphics, providing the reader with an understanding of the intent and the opportunities which the General Plan provides.

The third section concludes the report with design guidelines for the development of the General Plan. These guidelines set a level of design control for the phased development of the project by establishing the appropriate methods, concepts, elements and materials which will provide the integrity of Town Center.

Project History

The need for a viable commercial center for Foster City has been recognized as long ago as the incorporation of Foster City in 1971. The revised General Plan of 1971 allocated the area of the present Town Center site for town center use. Furthermore, the Land Use and Circulation Element of the Foster City General Plan adopted in 1974, on page 44, stated

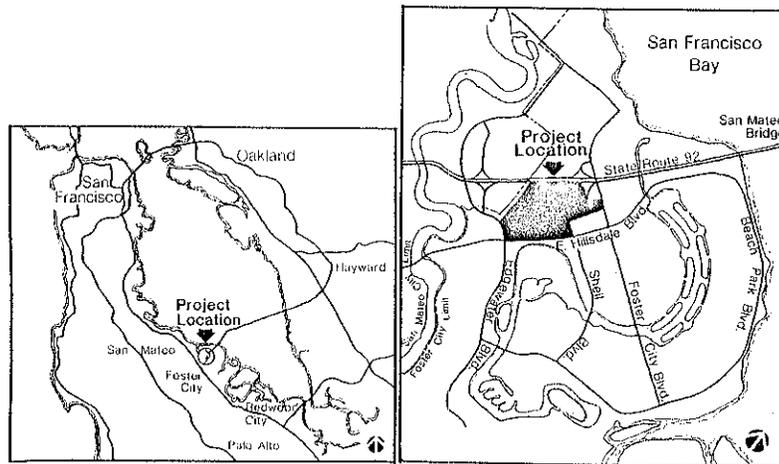
as one of its objectives to, "establish a commercial Town Center area as the hub of the City's commercial community in order to strengthen the economic base of the city and to provide a convenient, safe and attractive location for the functions that are unique to a central commercial area."

Transpacific Development Company has been involved in the planning and design of the Town Center for Foster City since 1979. The present plan has been developed in response to a reevaluation of a previous General Plan approved for the site by the Planning Commission in June, 1981. The present plan was initially submitted to the Planning Commission on April 26, 1983. Subsequent refinement and clarifications have taken place to produce a truly superior plan which will make Town Center a unique development for the City of Foster City, both aesthetically appealing and financially viable.

Project Location

The Town Center site is a 100 acre parcel within the jurisdiction of the City of Foster City, in San Mateo County, California. Situated on the San Francisco Peninsula it is within regional access of the major metropolitan centers of San Francisco, Oakland and the Santa Clara Valley via US 101 and State Route 92 (the San Mateo-Hayward Bridge).

The site is centrally located within the City of Foster City with easy access to highway routes US 101 and State Route 92. The major boulevards of Foster



Regional Context

Project Location

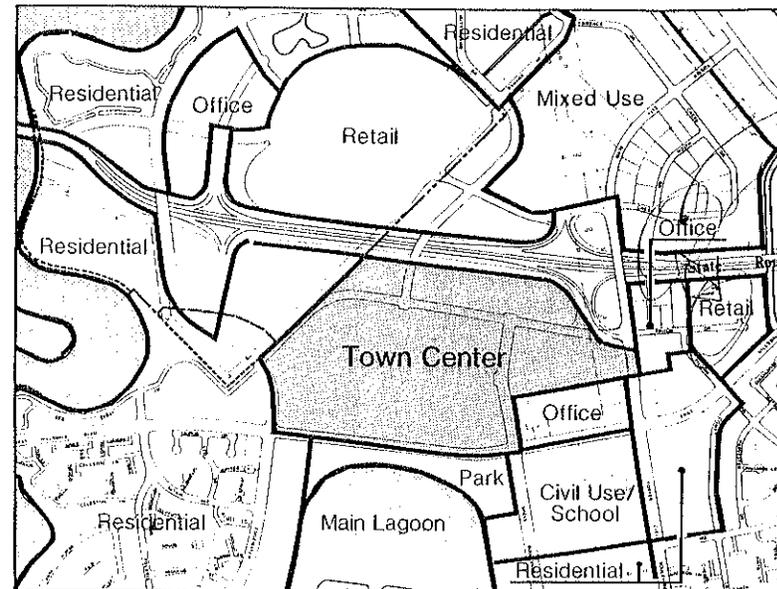
City Boulevard, Shell Boulevard, Edgewater Boulevard and East Hillsdale Boulevard form the edges of the parcel, providing ready access for local residents as well.

Surrounding Land Use

The Town Center site is physically separated from most surrounding land uses by State Route 92 and major arterials. These land uses are quite varied in scope and include retail, office, residential, recreational, civic and light industrial.

Retail and office developments exist to the east of the site across Foster City Boulevard. Office buildings are situated along East Hillsdale Boulevard between Shell and Foster City Boulevards contiguous to the southeast corner of the site. Apartment and town-home residential uses occur to the southeast diagonally across the Foster City Boulevard and the East Hillsdale Boulevard intersection. The Foster City Civic Center is in close proximity, located to the southeast of the site across the East Hillsdale Boulevard and Shell Boulevard intersection.

The Town Center site fronts Leo J. Ryan Park and the Main Lagoon, directly across East Hillsdale Boulevard to the south, providing an attractive visual and



Surrounding Land Use

recreational dimension to the area. Residential condominiums also occur to the south on the opposite side of the Main Lagoon. Residential use consisting of apartments, townhouses and condominiums occur to the southwest of the site across both East Hillsdale Boulevard and Edgewater Boulevards. In addition, a residential condominium project is being planned immediately northwest and contiguous to the site within both the City of Foster City and the City of San Mateo.

A major regional shopping center, The Fashion Island Mall, along with adjoining office development is located northwest of the site, across State Route 92, within the jurisdiction of the City of San Mateo. Vintage Park, a mixed use development, is being proposed north of the site across State Route 92, within the jurisdiction of Foster City. Extensive development of office and research and development-oriented structures exist to the northeast of the site diagonally across the State Route 92 and Foster City Boulevard interchange.

Project Goals

A series of goals have been defined which promote a town center development consistent with the aspirations set forth in the Foster City General Plan of 1974. The goals have served as guidelines by which the Town Center project has been planned. These General Plan goals are listed and described below:

1. Provide a distinct identifiable image for Foster City.

this is debatable

Foster City is a low-scale community of residential and commercial developments. As such, it lacks an identity which distinguishes it from other communities on the San Francisco Peninsula. A clear legible urban form will be instrumental in accomplishing this goal.

2. Provide for the community's needs.

The town center concept is an integral element of the General Plan. This centrally located site has historically been envisioned by the local community to be the urban core of Foster City. It is the intention of this plan to bring together the variety of uses that will sustain the level of activity which embodies an urban center environment.

3. Establish a network of open spaces.

To establish an urban character for Town Center, the suburban vehicle-oriented nature of Foster City needs to be augmented by a network of open spaces which would provide the link for the various elements of Town Center. This would serve to unify the entire development, further creating an identifiable image and place for Foster City, and promoting this urban experience for Town Center.

4. Provide a focal point for the community.

In addition to recognition on a regional and local community scale, a project of this magnitude should possess a landmark focal point. Therefore, as another objective of the plan, a focal open space is to be established in the tradition of a "town square". This will serve as a meeting place where a confluence of activities will occur for residents and visitors alike.

5. Integrate Town Center into the existing context of development.

*has this been achieved?
("Gemco")*

The surrounding development needs to be considered in order to integrate Town Center into the surrounding community and achieve an overall cohesiveness for Foster City. Architectural and landscape elements of the plan can aid in making a smooth transition from existing patterns of development to the scale and density of the Town Center nucleus.

6. Provide a framework for future growth.

Due to the size, complexity, and duration of the project, there is a need to establish a circulation system and set of developmental guidelines for future growth. These guidelines and movement patterns will aid in maintaining the integrity of Town Center with the surrounding community and within the project itself throughout the phases of the development.

7. Develop a plan which is economically feasible and receives market acceptance.

For Town Center to become a reality, it must be efficiently planned. Furthermore, Town Center must be designed to respond to

the marketplace. Each element of the plan (housing, office, retail and hotel) is to be conceived to offer quality amenities, convenience and special features while providing a variety of opportunities to fulfill market demands.

8. Enhance the human comfort of outdoor spaces by minimizing harsh climate conditions.

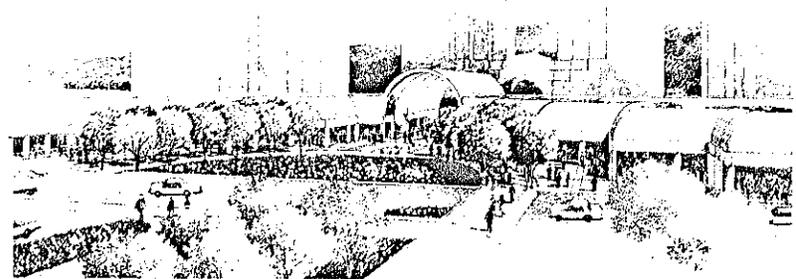
Foster City is situated such that winds are experienced out of the north and northwest. For the urban concept of a network of open spaces to be viable, these climatic conditions need to be addressed.

of Development
This report continues with a detailed description of the General Plan followed by a section on the Design Guidelines to be used in the implementation of the General Plan *of Development*.

General Plan of *Development*

Overview

Town Center is a master planned, 100 acre mixed-use development. It is envisioned upon completion to encompass approximately 1.4 million square feet of office space, 285,000 square feet of retail shopping area, a 300-room hotel and a residential component ranging between 410 - 500 dwelling units. These elements focus on a central landscaped open space.



Main Entry - Town Center

The Plan receives its strength and integrity of design from the creation of a "Town Center" theme. This theme develops from the provision for a community focal point and a balanced organization of plan elements. A mixture of uses and the integration of diverse building elements through a network of open spaces are two other important aspects that promote this "Town Center" theme. These points are discussed further in the following description of the General Plan.

*"where are
"open spaces"
besides central
"park"?"*

Town Center, through its mixed-use plan, embodies many of the project goals described in the introduction. An identifiable image, a focal point, community needs, economic feasibility and market acceptance are fulfilled by Town Center's mixed-use urban center approach to the development of the site.

deleted

The network of open spaces evolves as a direct result of the need to relate plan elements to each other while creating a "people-place" at Town Center. The relationship to surrounding developments is achieved through a build-up in massing and height of structures towards the project center; while the goal of providing the framework for future growth is directly tied to the organization of the Plan which creates a classic urban "grid".

In summary, Town Center is to be the core of Foster City. As a city core, it will help to promote social and commercial interaction for Foster City. The development, as envisioned in this General Plan ^{of Development} ~~submit~~-tial, can offer the opportunities and choices which warrant this wide-ranging appeal.

This section continues with a discussion on those concepts which helped mold the overall General Plan design, followed by a detailed description of the various plan elements. All information regarding these plan elements, as well as the various technical and specific issues which follow, are conceptual and approximate in nature. These specific issues which are addressed are: site use, site dimensions, site circulation, site landscape, site signage, and phasing and implementation.

Design Elements

A Community Focal Point

Since Town Center is envisioned as the urban core of the Foster City community, it should be recognizable as a focal point and be seen from a distance. The existing pattern of development in the surrounding area is one of predominantly low rise residential and commercial structures. Town Center, in contrast, will project an image on the skyline which builds in height and density from the project periphery to the project center.

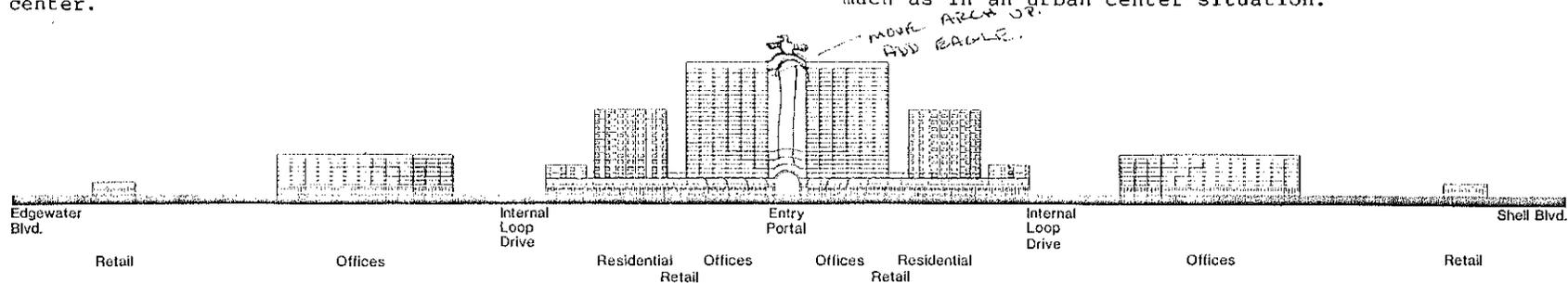
Seen from East Hillsdale Boulevard, the flanking low-rise office buildings provide an anchor to the internal drive entries and frame the axial view of the retail canopy, entry portal, and the Town Center central open spaces. In the zone beyond, the building height and massing increases to the midrise elements of the core and culminates in the center office towers, the visual apex of the urban core development. The massing and axial placement of architectural forms gives the development its most significant landmark qualities. The overall impression is meant to bring a distinct identity, not only to the project, but to the entire Foster City area.

Although the Town Center development will be an identifiable and dominant feature on the landscape, it does so in a manner that is compatible with its surroundings. This is accomplished by making a transition in scale and by using manageable building elements. Even the largest structures within the project are not of a scale unfamiliar to the area. The unique character of Town Center comes from the special way the elements are blended.

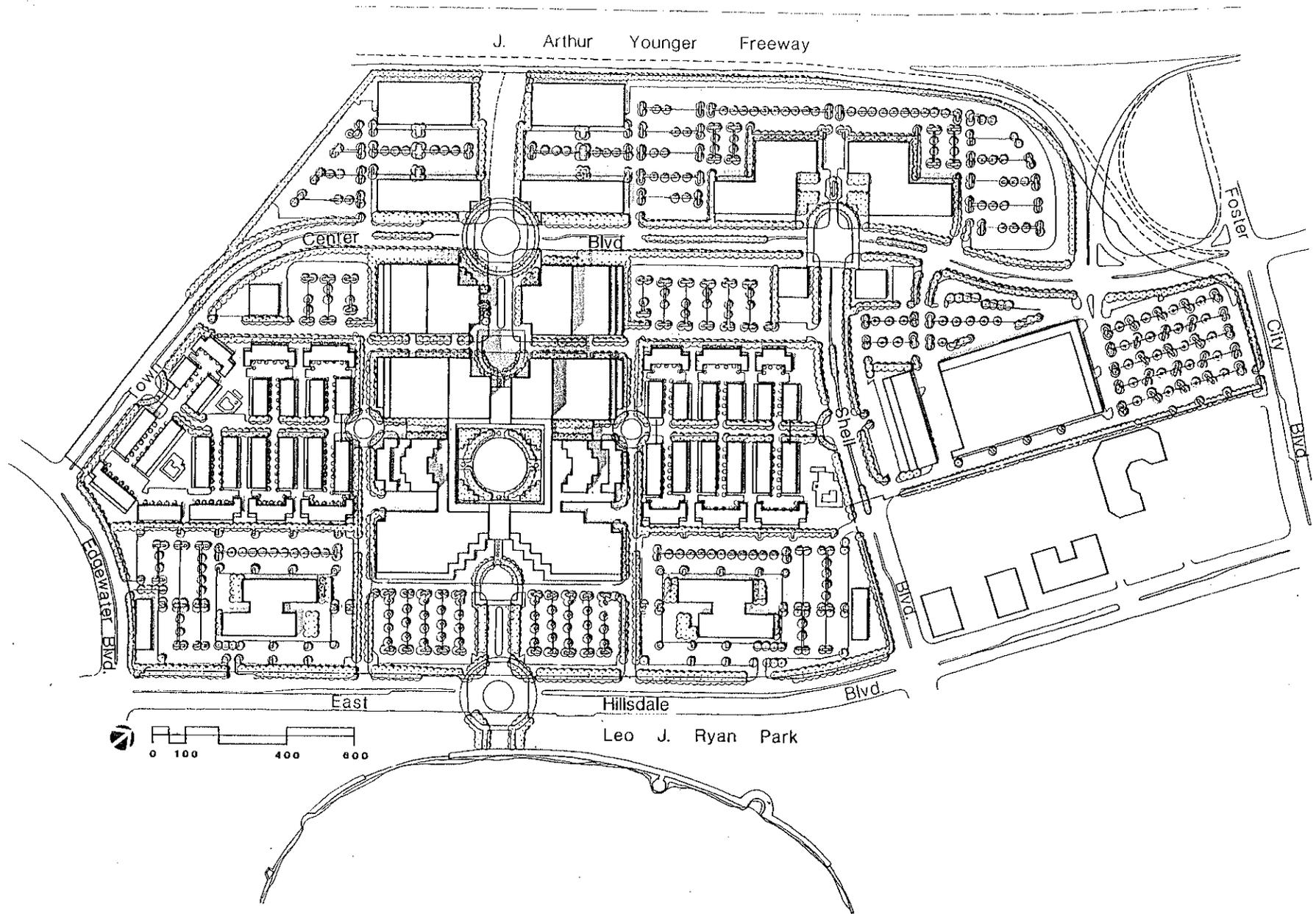
Plan Organization

A balanced, symmetrical approach to the placement and orientation of retail, residential, and office buildings is used to create a classic urban "grid" layout which promotes the "Town Center" theme, and contributes to the project on a number of levels. Based on this placement and orientation of these structures, the grid layout becomes the system of building plots and rights-of-way which will serve to guide future development.

On the overall image level, this "grid" pattern establishes the order of architectural volumes and open spaces that gives Town Center its distinctive urban character. Buildings are organized along symmetrical lines with building edges brought to a consistent line much as in an urban center situation.

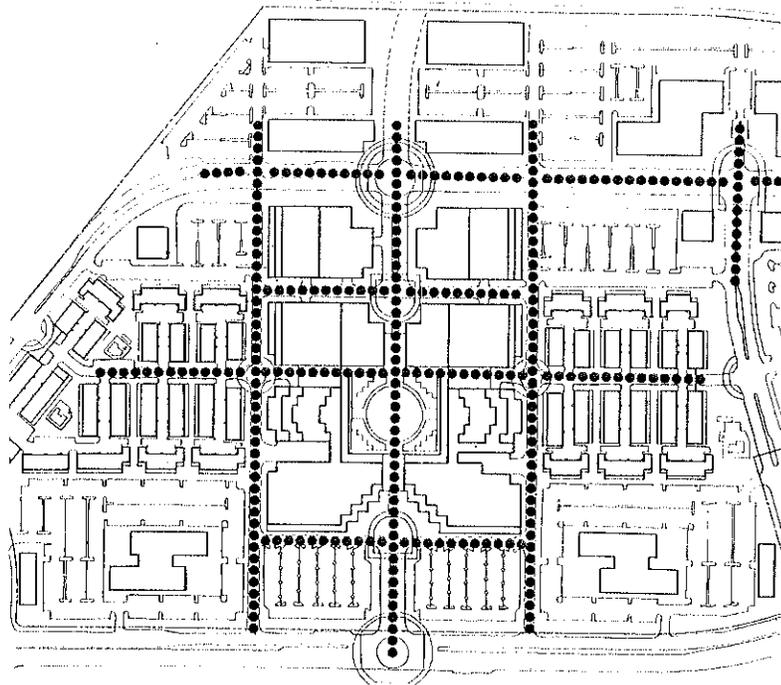


East Hillsdale Blvd. Elevation



Illustrative Site Plan

This grid layout also lends itself to a well-defined internal circulation pattern. An understandable pedestrian and vehicular circulation system enhances and fortifies Town Center's urban image while heightening the convenience and success of Town Center.



Grid System

In addition, the establishment of these rights-of-way and property line guidelines will help to facilitate the phased development of individual elements.

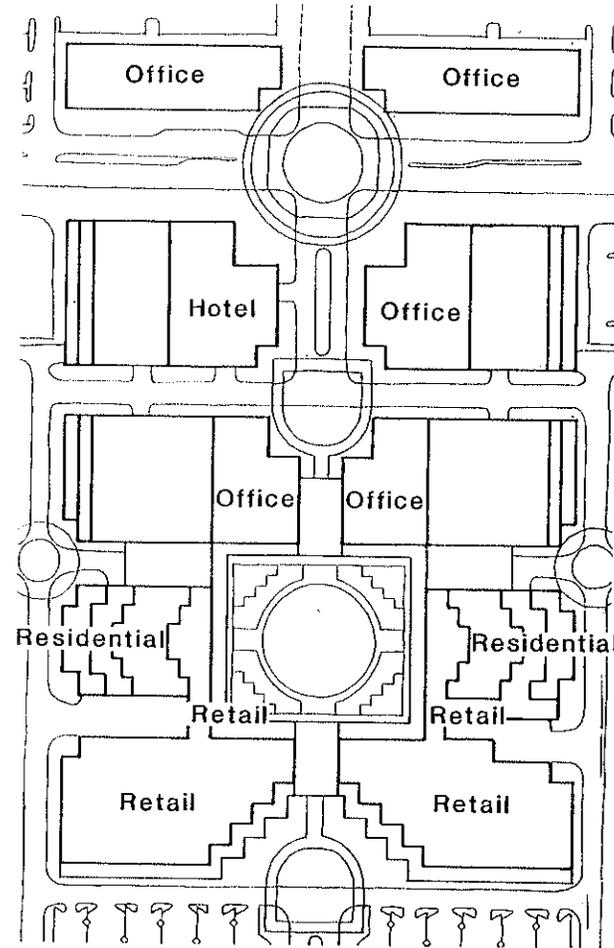
Multi-Use Core

The Town Center core is a confluence of retail, office, residential, hotel, and open space uses. The interplay of these uses is achieved by clustering together discrete building elements of diverse uses, thus creating a vibrant city core to Foster City, indeed an active "people-place."

The core retail is a combination of large neighborhood convenience establishments facing East Hillsdale Boulevard, with smaller shops, cafes and eateries surrounding the central open space and flanking the entry portals.

This commercial area around this central open space is primarily service oriented to meet the needs of the office, hotel and residential elements.

The core residential is a pair of mid-rise structures flanking the east and west edges of the open space while a pair of mid-rise office towers form the northern edge. Three additional office structures and a hotel form the northern entry to the core, where the Central Promenade intersects Town Center Boulevard.



Multi-Use Core

Parking within the core is primarily incorporated into parking structures to consolidate parking and to allow the added density of uses within this multi-use core. Parking is provided for residents of the mid-rise housing, and users of the office towers and hotel. Because the perimeter retail is dependent on the synergism of Town Center's core and flanking residential, the parking requirement for this element is to be minimal and may need to rely occasionally on adjacent parking facilities. Parking for the large retail uses facing East Hillside Boulevard is provided in landscaped surface parking areas.

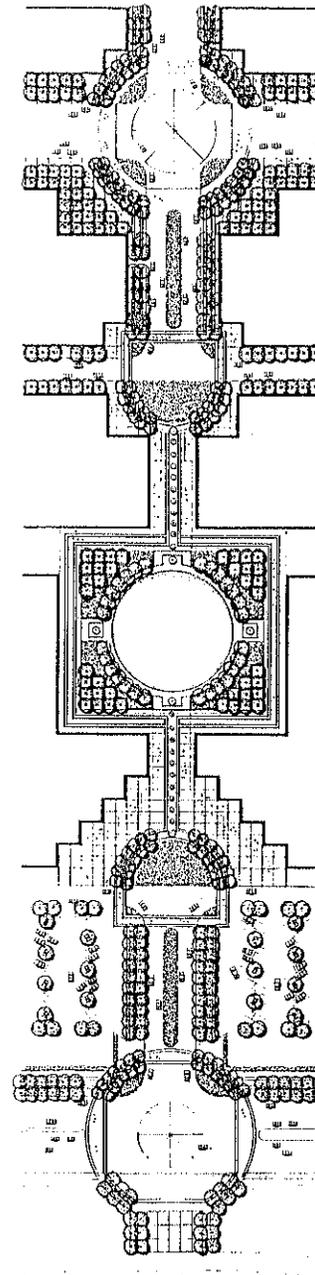
As described, the core components account for the majority of space within the total master plan. These components in their compact arrangement around the project's major north-south axis, create the density of uses and focus of activities that will make the Town Center a vibrant "people place".

Open Space Network

The "Town Center" theme develops not only out of the buildings which make up the project, but also from the interaction of people who live, work and visit the development. In order to provide the opportunities for this social interaction to occur, as well as to relate individual building elements of the Plan, a network of open spaces has been developed. This network centers on the major north-south open space corridor of the Plan formed by the axial arrangement of buildings within the central core.

The open spaces are as much a unifying factor of the plan as are the structures that form it. The spaces of this Central Promenade in turn become the open space amenities. The Promenade provides a dramatic north-south view and access corridor through the entire site, from the J. Arthur Younger Freeway to Leo J. Ryan Park and the Main Lagoon. This sequence of open spaces is connected with a cohesive landscape treatment which includes special intersections and pedestrian crossings, water elements and special plantings.

Several places of significance combine to create this central pedestrian promenade. The major gateway intersection of Town Center Boulevard at the north entry drive signals the northern gateway of this open space corridor, while the entry plaza at the mid-rise central office towers signals arrival at the northern portal to the central landscaped open space.



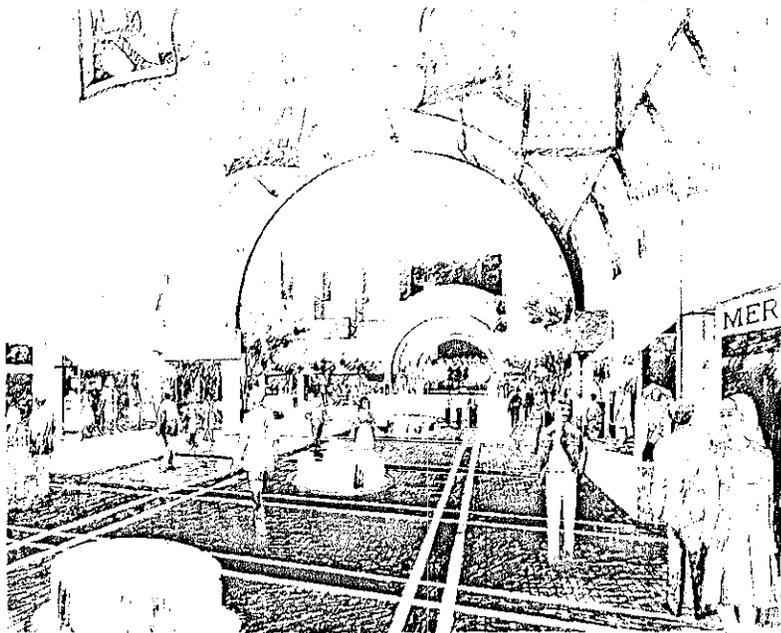
Central Promenade

The Central Promenade continues through this central open space to a southern portal at a retail entry plaza which is semicircular in form. The major intersection of East Hillsdale Boulevard and the south entry drive provides the southern gateway of this open space corridor, balancing with the northern gateway. The Central Promenade terminates in Leo J. Ryan Park at an overlook plaza facing the Main Lagoon.

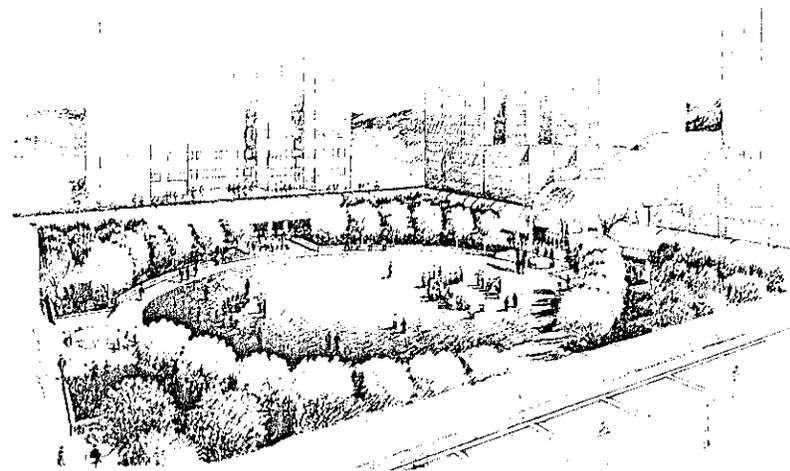
Central Open Space

The central landscaped open space is the highlight of this pedestrian promenade, as well as the focus of the entire Town Center development. It gains further prominence from being at the intersection of this major north-south open space corridor with the secondary east-west corridor which leads to the flanking mid-rise residential element and townhouse developments.

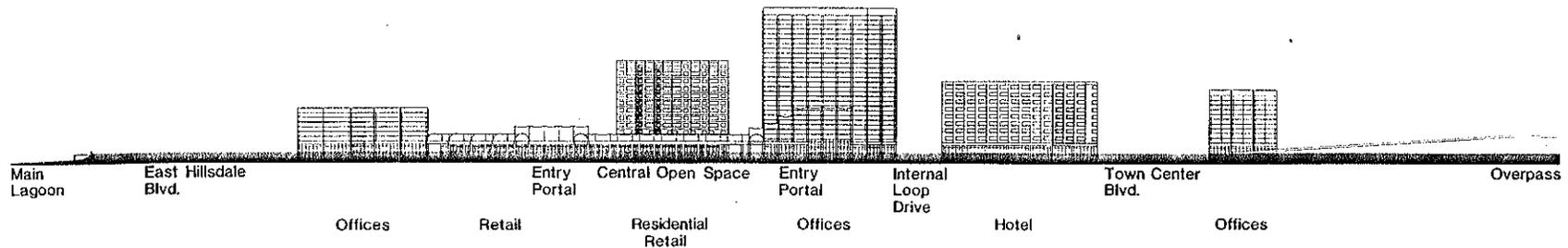
This central open space is defined by the mixed-use buildings which form the central core. In keeping



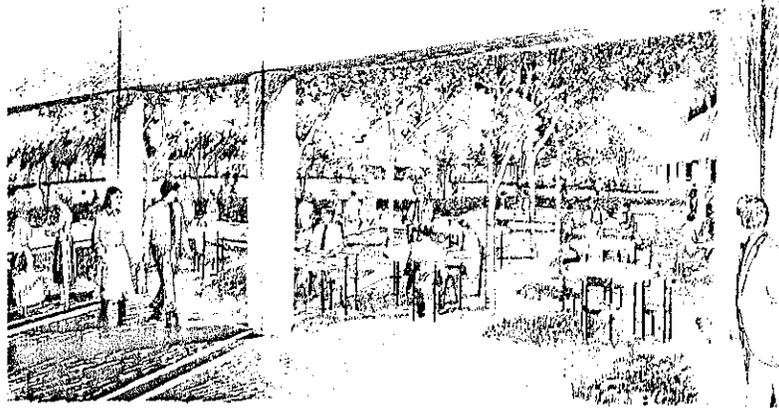
View Through South Entry Portal



View into Central Open Space



North-South Site Section

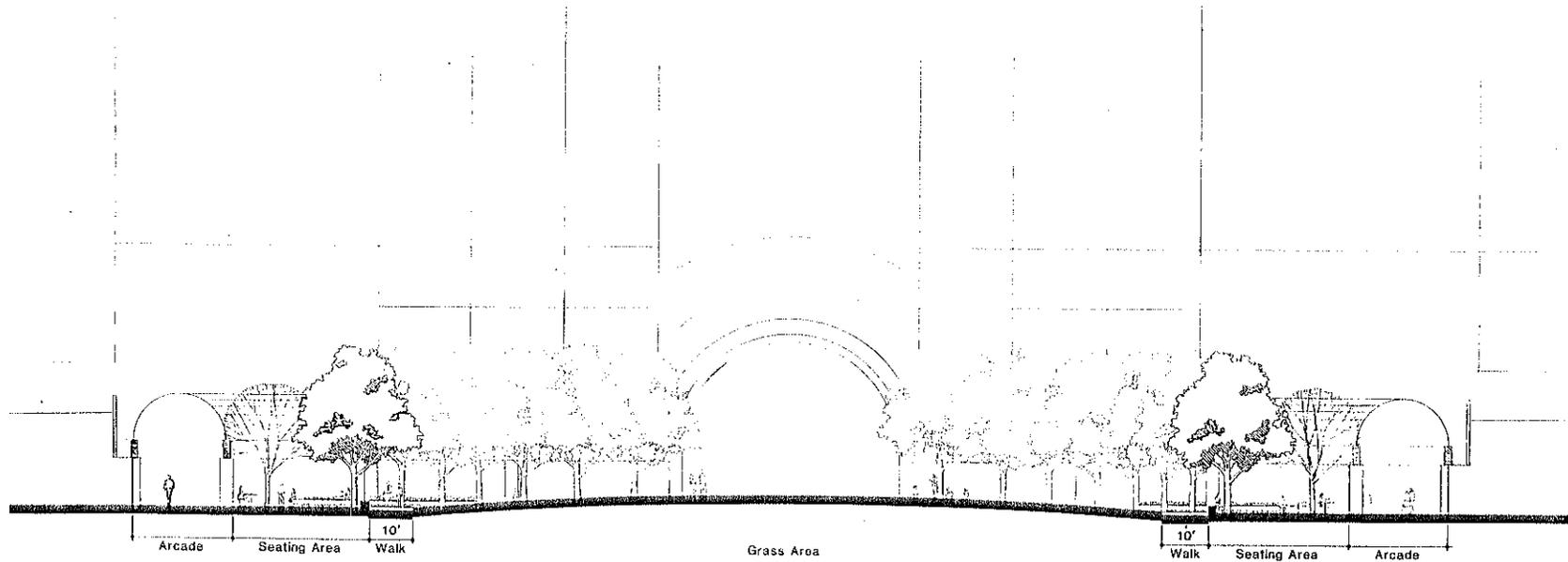


View into Central Open Space -Corner Seating Area

with the urban "grid" concept, it is formal and symmetrical in its layout. The design gains its elegance from the geometric simplicity of a circle inscribed within a square formed by the arcade and core buildings. This circular space forms the Town Center "Green", in which a wide range of social activities can take place, from large organized gatherings to informal and impromptu activities of a much smaller scale. A retail element has been designed to occur around this open space area providing the stimulus for pedestrian activity and social interaction, and inviting the pedestrian into the area. Spaces around the perimeter of the central open space and adjacent to this retail area are more intimate in scale to provide a contrast with the large open area of the "Town Green". Special plantings, provisions for seating areas, water features, and the activity generated by the retail combine to give these peripheral spaces a distinct Town Center flavor.

East-West Open Space Corridor

The central landscaped open space sits at the intersection of the two open space corridors of the development, the north-south Central Promenade and the secondary east-west open space corridor. This east-west corridor links the central open space to the residential



Section A - Central Open Space

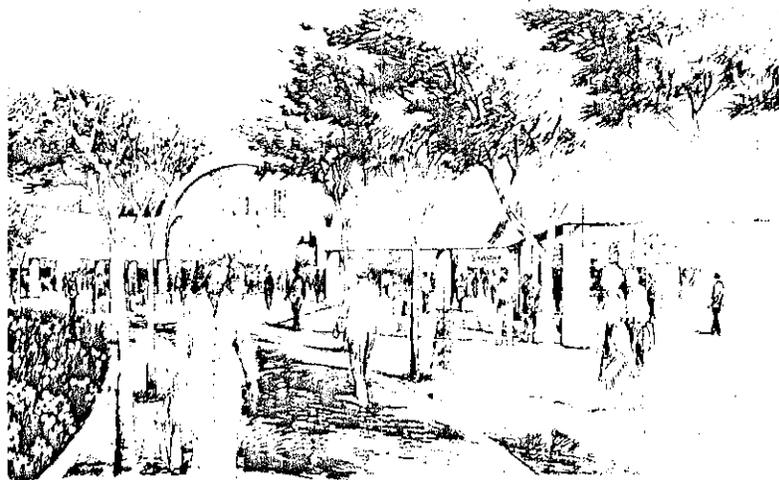
areas which flank each side of the central core, both visually as well as physically.

The concept of a cross-axis to the major north-south corridor is an important factor in the system of pedestrian spaces within Town Center. The ability to enter the central open space from four directions, instead of two, helps to increase the level of people activity within the central core. Furthermore, this east-west pedestrian link guarantees uninterrupted passage for residents of Town Center throughout the development.

Pedestrian Connections

As part of the overall open space network of Town Center, specially treated intersections are proposed for major crosswalks and gateways. These intersections serve as distinctive visual features which help delineate crosswalks and entry points to various areas in the project and establish a family of forms which enrich the open space network.

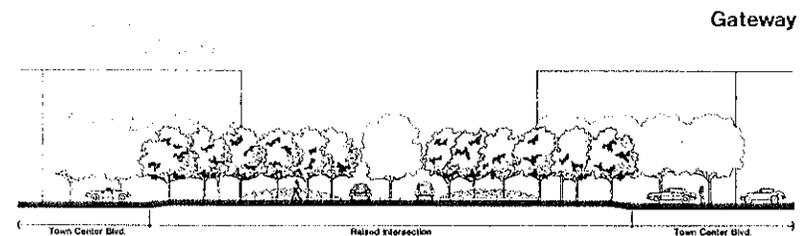
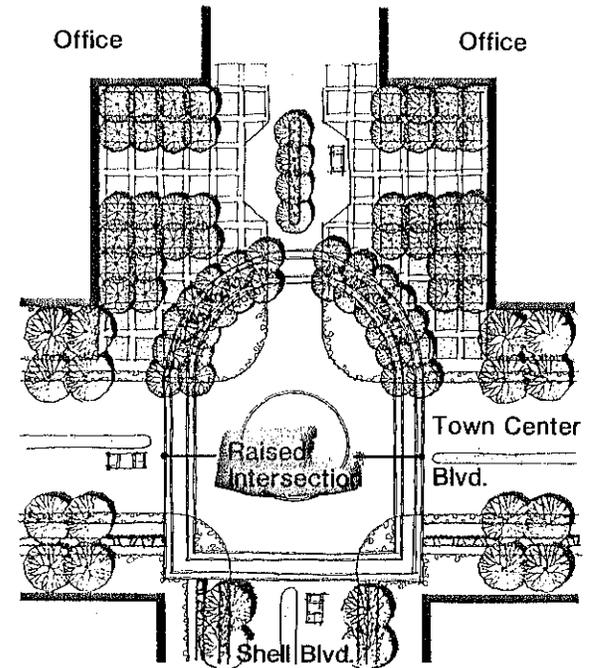
The major gateway intersections are part of the special places along the central pedestrian promenade. These intersections are characterized by special pedestrian paving and large panels of groundcover accenting the corners while still allowing for visibility. Also enhancing the corners are accent trees planted to reinforce the intersection form.



View of South Entry Plaza

Along the Central Promenade these intersections contain large masses of flowering groundcover and double circular rows of accent trees. Together these form a strong visual gateway defining the pedestrian entry into the central open space/core area at a scale appropriate to the pedestrian.

A minor gateway/crosswalk occurs at the northern terminus of Shell Boulevard leading into the two office buildings. This pedestrian connector is the focus of views down Shell Boulevard and forms a visual gateway to the office complex. The smaller scale of the crosswalk coupled with groundcover and accent trees define this intersection.

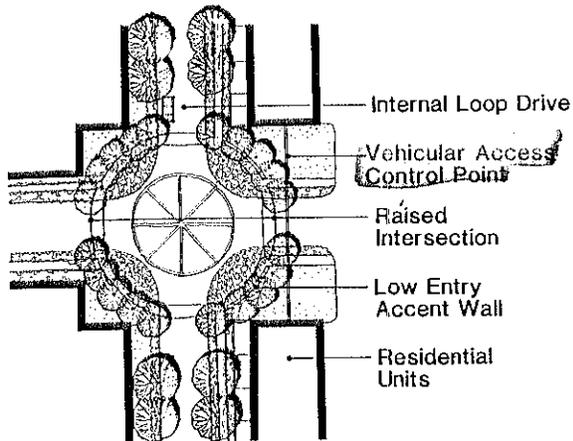


Section B - Town Center Blvd./Shell Blvd. Intersection

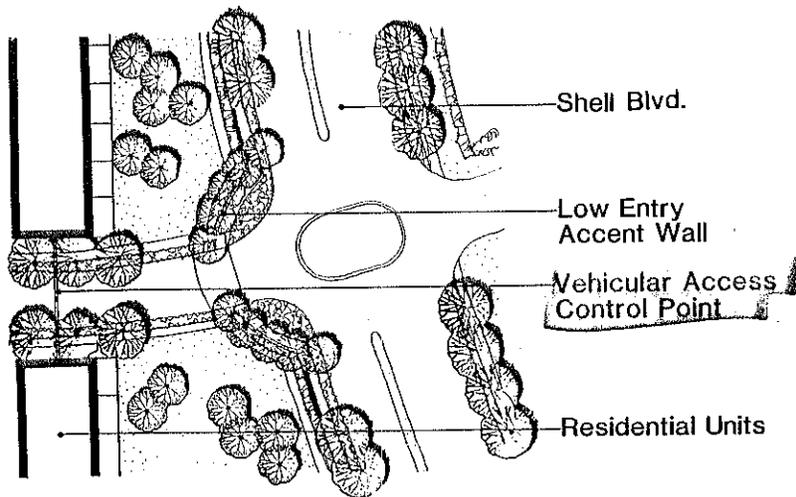
The remaining minor gateway/crosswalks serve the entries into the residential parcels. The smaller scale intersections between each of the two residential streets and the internal drives form strong pedestrian and visual links between the areas. The intersections into the two residential parcels at Town Center Boulevard and Shell Boulevard are half circle forms with a similar treatment as the two other residential intersections.

TYPE OF CONTROL!!!

These four intersections identify vehicular-controlled gateways into the residential areas and contain low accent walls that may serve as entry features. As with the gateway at Shell Boulevard, these intersections are smaller in scale than the major gateways along the central Promenade. Nevertheless, through the use of similar circular forms and landscape treatments, all of these intersections and gateways add to a distinctive signature of Town Center.



Residential Gateway at Internal Drive

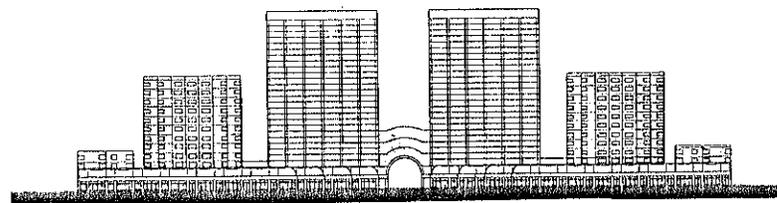


Residential Gateway at Perimeter Street

Canopy and Portals

The open space network not only includes this special family of gateways and crosswalks but also includes the spaces formed by the canopy and portals. This continuous canopied arcade links together the buildings of the central core, providing a linear open space corridor onto which the retail units behind the arcade open. This space provides the connection into the central landscaped open space while the arcade becomes the common architectural theme threading throughout the core of Town Center development.

The canopy begins at the retail edge that faces onto East Hillsdale Boulevard, and runs the full length of the east and west flanking buildings. At the project center, the canopy steps back, forming a plaza and the southern entry portal to the central open space. Within this open space area the arcade continues around the perimeter edge, setting the character of the retail facades within. At the northern side of the central open space, the canopy connects to another entry portal, between the mid-rise office towers. These portals at the north and south entries to the central open space form grand spaces, reinforcing the civic nature of the project's central axis.



Elevation - Town Center Canopy

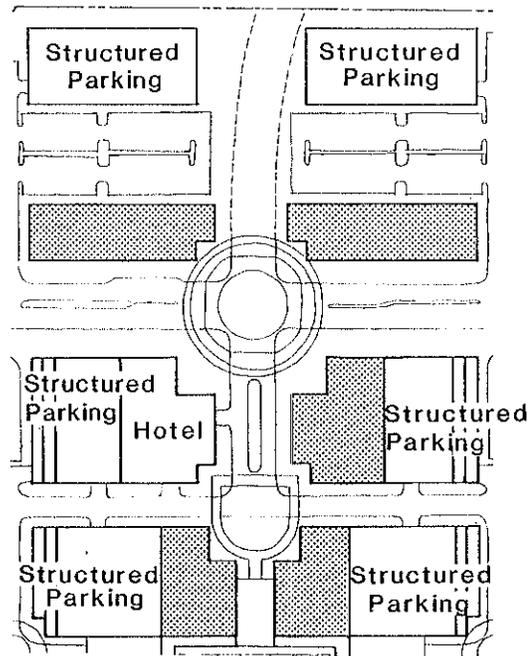
Within the canopied arcade, the space can be used for an extension of the retail uses, providing movable display space which will add color and interest. Additionally, seating, planting and accent elements such as banners and signs will help to create a lively market atmosphere for this pedestrian pathway.

Uses

Office

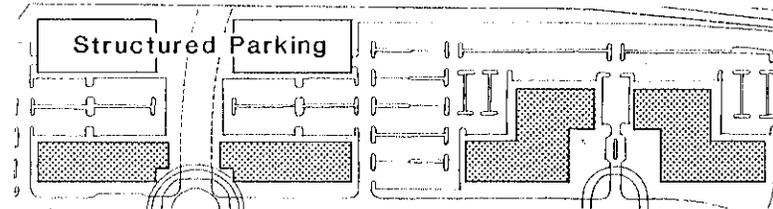
The office portion of the project will be developed in several structures varying in height. For the purposes of discussion, the office buildings can be grouped into three categories; the central core, the intersection of Town Center and Shell Boulevard, and East Hillsdale Boulevard.

The plan depicts varying mid-rise office structures in the central core area. The largest are the two towers at the very center of the project. Each will be in excess of ten stories in height. The office building to the east of the hotel is approximately six floors. The two remaining buildings in the core are located at the northern intersection of Town Center Boulevard, and will be approximately six stories in height. In order to achieve the higher density of the central core, these office buildings will incorporate structured parking.

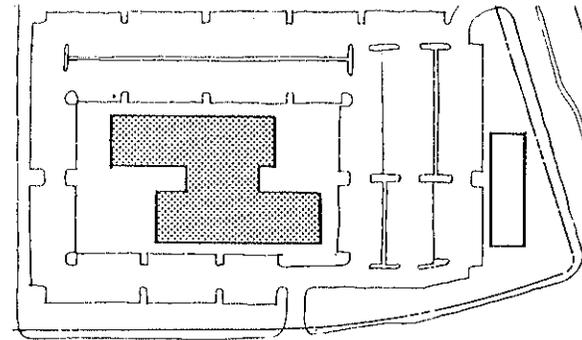


Core Office

The two office buildings at the intersection of Town Center Boulevard and Shell Boulevard are approximately three stories each. The two East Hillsdale buildings are each approximately 135,000 square feet and three or four stories.



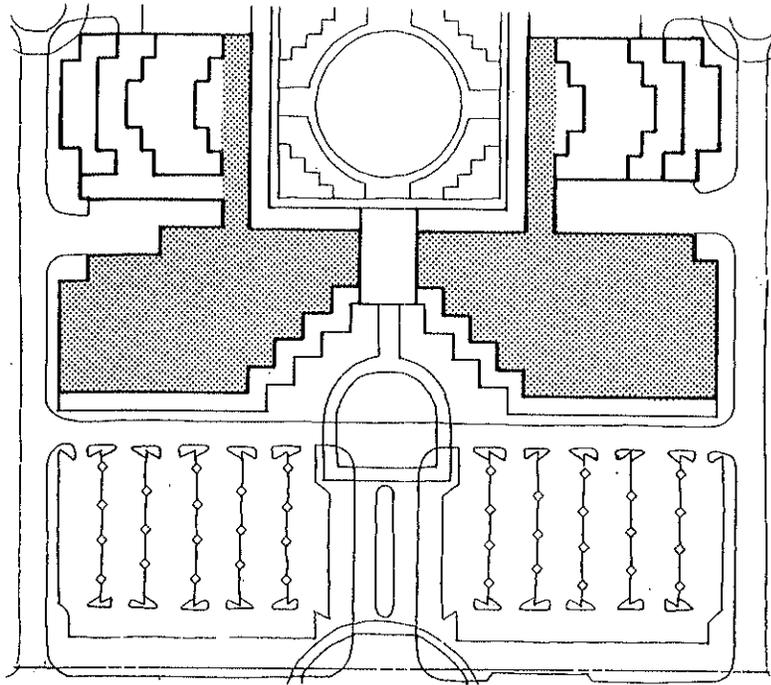
Town Center Blvd. Offices



East Hillsdale Blvd./Shell Blvd. Office

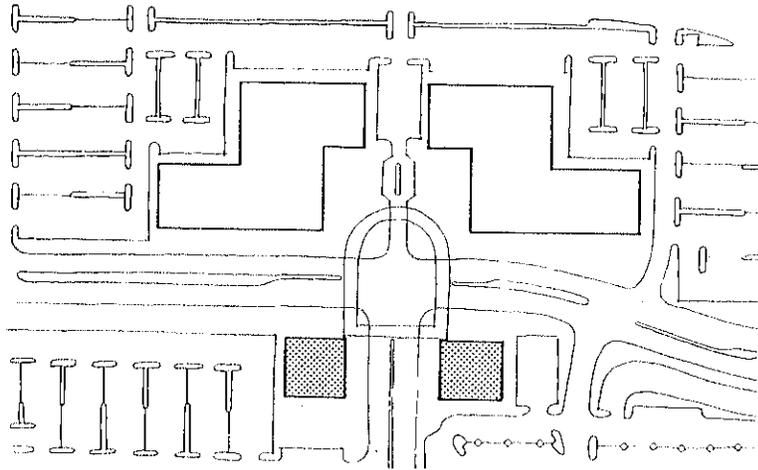
Retail

The retail component of the plan consists of a variety of types. The retail spaces fronting onto East Hillsdale Boulevard are of a neighborhood convenience-type retail. They combine a major market, a major drug-store, and small variety shops. This total area is approximately 95,000 square feet. The shops surrounding the central open space will be primarily small scale support facilities for the residential, hotel and office users. This retail element totals approximately 24,000 square feet. The East Hillsdale and central open space retail are unified by means of the canopied arcade. Through this architectural element, the individual parts take on a larger purpose of urban form and content.



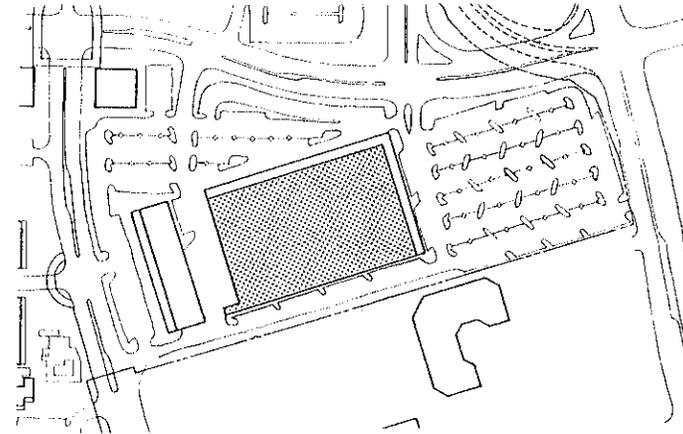
East Hillsdale Retail

The retail facilities located at the project's peripheral streets and intersections will be within a range of approximately 5,000 to 8,000 square feet each.



Town Center Blvd./Shell Blvd. Retail

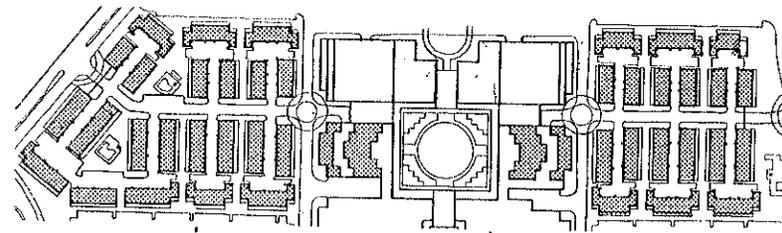
The retail facility at the northeast portion of the project will be a regional retail facility of about 105,000 square feet. In addition, a general purpose retail facility of approximately 22,000 square feet will be situated immediately west of this major retail facility. This location is well suited for this type of use. In addition, an existing easement along the eastern portion of this site lends itself to the parking needs required by this type of regional-draw retailer, as well as providing area for visual landscape buffers.



Regional Retail

Residential

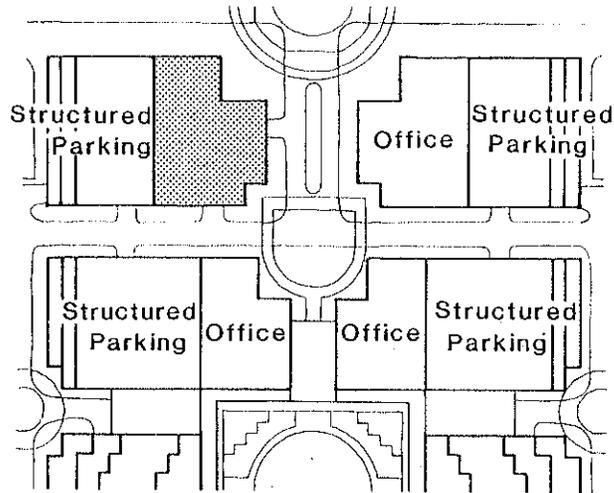
The residential community flanking the central landscaped open space to the east and west is a mix of mid-rise residential units (200-250 units) and townhouses (210 - 250 units). The mid-rise units are set on a podium level with structured parking below. The townhouse site plan branches out from the Town Center with major east/west circulation streets and minor north/south vehicular and pedestrian courtyards.



Residential Elements

Hotel

The hotel facility of approximately 300 rooms is located on the northwest edge of the Central Promenade where the Promenade intersects Town Center Boulevard. This intersection is conceived as a landscaped node and an integral part of the project's central sequence of spaces. In this location, the hotel will have highway visibility as well as contribute to and benefit from the urban vitality of the Central Promenade's confluence of uses.



Hotel

Site Use

This section on site use highlights many elements of the General Plan. The following tables give figures for land use in terms of acreage for: the different types of site coverage such as buildings, landscaping, internal circulation and parking areas; the different land uses broken down into office, retail, residential, and hotel uses; and a summary of the parking spaces present on the site.

The accompanying site-use plan illustrates the relationships of the different uses to one another and the overall relationship with parking, circulation, and open space. For purposes of this report, the figures contained in the following tables and summaries, as well as throughout the report, are approximate only. Actual amounts will be specified upon the use permit application.

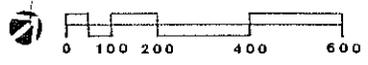
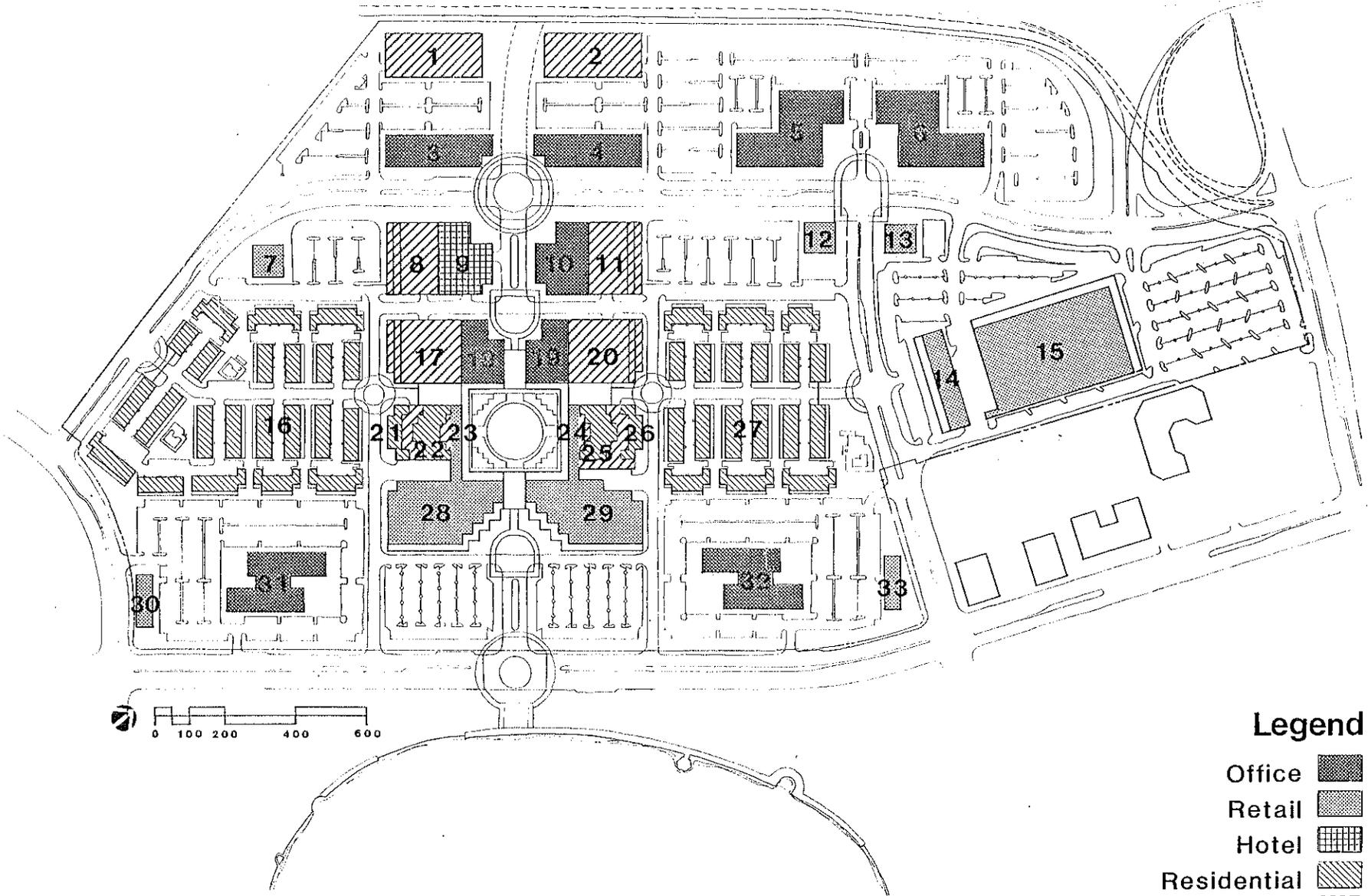
Site Use Summary

| USE | AMOUNT | AREA |
|---------------------------------------------|-------------------|---------------------------|
| Office | 1,380,000 sq. ft. | 44.7 acres |
| Retail | 287,000 sq. ft. | 25.8 acres |
| Residential | | |
| Mid-Rise Units | 200-250 units | 2.7 |
| Townhouses | 210-250 units | <u>15.8</u> |
| Total Units | 410-500 units | 18.5 acres |
| Hotel | 300 rooms | 1.9 acres |
| Corridor Landscaped Open Space ^a | N/A | 2.5 acres |
| Public Roads | N/A | <u>6.3 acres</u> |
| | | Total = 99.7 acres |

^a The Corridor Landscaped Open Space includes the 1.2 acre central green area and another 1.3 acres of landscaped area in the central corridor.

Acreege Summary

| Type of Coverage | Area |
|------------------------------------|-------------------|
| Buildings | 24.8 acres |
| Landscaping and Central Open Space | 25.4 acres |
| Public Roads | 6.3 acres |
| Parking & Circulation | <u>43.2 acres</u> |
| SITE TOTAL | 99.7 acres |



- Legend**
- Office 
 - Retail 
 - Hotel 
 - Residential 
 - Parking Structure 

Site Use

Office Use Summary

| Building Number | Floor Area (Sq. Ft.) | Height (Ft./Floors) | Total Parking (Stalls) | Area (Acres) | | |
|-----------------|----------------------|---------------------|------------------------|--------------|-------------|-----------------------|
| | | | | Building | Landscape | Parking & Circulation |
| 1 | N/A | 20/2 | a | 1.3 | 1.9 | 2.0 |
| 3 | 150,000 | 80/6 | 495b | | | |
| 2 | N/A | 20/2 | a | 3.3 | 4.3 | 7.6 |
| 4 | 150,000 | 80/6 | 495b | | | |
| 5 | 125,000 | 40/3 | 412 | | | |
| 6 | 125,000 | 40/3 | 412 | 1.1 | 0.7 | 1.5 |
| 10 | 120,000 | 80/6 | 396b | | | |
| 11 | N/A | 30/4 | a | 1.3 | 0.7 | 1.2 |
| 17 | N/A | 30/4 | a | | | |
| 18 | 220,000 | 190/12 | 726b | 1.3 | 0.7 | 1.2 |
| 19 | 220,000 | 190/12 | 726b | | | |
| 20 | N/A | 30/4 | a | 0.8 | 1.5 | 5.0 |
| 31 | 135,000 | 40-54/3-4 | 446 | | | |
| 32 | 135,000 | 40-54/3-4 | 446 | 0.8 | 1.5 | 5.0 |
| Totals | 1,380,000 | | 4,554 | 9.9 | 11.3 | 23.5 |

- a. Structure parking facility.
b. Mixed surface and structured parking.

Retail Use Summary

| Building Number | Floor Area (Sq. Ft.) | Height (Ft./Floors) | Parking (Stalls) | Area (Acres) | | |
|-----------------|----------------------|---------------------|------------------|--------------|------------|-----------------------|
| | | | | Building | Landscape | Parking & Circulation |
| 7 | 8,000 | 15/1 | 80 | 0.2 | 0.4 | .45 |
| 12 | 8,000 | 15/1 | 80 | 0.2 | 0.7 | .45 |
| 13 | 8,000 | 15/1 | 810 | 3.1 | 2.3 | 7.4 |
| 14 | 22,000 | 23/1 | | | | |
| 15 | 105,000 | 23/1 | 24,000 | 0.3 | 0.2 | .9a |
| 23 | | 23/1 | | | | |
| 24 | | 23/1 | 464 | 2.3 | 1.5 | 4.2 |
| 28 | 50,000 | 23/1 | | | | |
| 29 | 46,000 | 23/1 | 80 | 0.3 | 0.3 | .45 |
| 30 | 8,000 | 23/1 | | | | |
| 33 | 8,000 | 23/1 | 80 | 0.3 | 0.3 | .45 |
| Total | 287,000 | | 1,594 | 6.7 | 5.7 | 13.4 |

- a. The central landscaped open space perimeter retail, which may include small eateries and shops, is primarily support services oriented to meet the needs of the surrounding office, residential, and hotel users. Therefore, the parking requirement for this retail element is minimal, and the parking ratios reflected herein are not applicable to this element.

Residential and Hotel Uses Summary

| Building Number | Type of Use | No. of Units/Rms | Height (ft./ Floors) | Parking (stalls) | Area (Acres) | | |
|-----------------|---------------------|------------------|----------------------|------------------|--------------|-----------|-----------------------|
| | | | | | Building | Landscape | Parking & Circulation |
| 8 | Parking | N/A | 30/4 | b, c | 1.2 | 0.5 | 0.2 |
| 9 | Hotel | 300 | 100/9 | 385 | | | |
| 16 | Townhouses | 115-135 | 25/2 | 260-305 | 3.1 | 2.3 | 3.3 |
| 21 | Parking | N/A | 24/2 | b | 0.8 | 0.2 | 0.35 |
| 22 | Mid-Rise Units | 100-125 | 120/10 | 150-190 | | | |
| 25 | Mid-Rise Units | 100-125 | 120/10 | 150-190 | 0.8 | 0.2 | 0.35 |
| 26 | Parking | N/A | 24/2 | b | | | |
| 27 | Townhouses | 95-115 | 25/2 | 215-260 | 2.3 | 2.7 | 2.1 |
| Totals | Hotel: | 300 Rooms | | 300 | 1.2 | .5 | 0.2 |
| | Residential: | 410-500 units | | 775-945 | 7.0 | 5.4 | 6.1 |

a. A Parking Requirements Study will be submitted, which will provide justification for the proposed residential parking counts. Actual parking allotment will be determined based on the parking study.

b. Structure parking facility.

c. 300 spaces of the 385 spaces are allocated for the hotel (Building 9).

Parking Summary^a

| USE | AREA | PROPOSED STALLS | CITY ORDINANCE |
|-------------|-------------------|-----------------|--------------------|
| Office | 1,380,000 sq. ft. | 4,554 | 6,900 |
| Retail | 287,000 sq. ft. | 1,594 | 1,435 ^b |
| Residential | 410-500 units | c | c |
| Hotel | 300 rooms | 300 | 300 |

a. A Parking Requirements Study will be submitted, which will provide justification for the proposed parking counts.

b. Based on retail use only

c. Parking counts depend on actual number of bedrooms. Parking provided shall be in accordance with the Parking Requirements Study.

Site Dimensions

The Site Dimensions Plan contains all critical circulation and easement dimensions. This plan includes all of the street widths, including surrounding streets and median strips, all critical turning radii, and dimensions for all easements, including the perimeter street landscaping easements.

Landscape Easements

Landscape Easements are described on the current Town Center Tentative Map. The easements measure 15 feet from the property line to the landscape easement line on either side of Town Center Boulevard, Shell Boulevard, the east side of Edgewater Boulevard, the west side of Foster City Boulevard and along the northern boundary of State Route 92. These landscape easement areas are augmented by the landscaped area within the road right-of-way. This landscaped area extends from the face of curb to the property line and is 6.5 feet for all streets listed above with the exception of State Route 92. The right-of-way along State Route 92 varies along its length adjacent with Town Center property and is approximately 14 feet. East Hillsdale Boulevard has a 29-foot landscape easement along its entire length of Town Center property. In combination with 11 feet of landscaped area in the road right-of-way, this perimeter landscaped area is 40 feet. The Landscape Easement Table below presents these landscape areas in detail.

LANDSCAPE EASEMENTS AND AREAS TABLE

| <u>Street</u> | <u>Landscape Easement</u> | <u>Right-of-Way Landscape Area (a)</u> | <u>Total Landscape Area</u> |
|---------------------|---------------------------|----------------------------------------|-----------------------------|
| East Hillsdale Blvd | 29 feet | 11.0 feet | 40.0 feet |
| Edgewater Blvd | 15 feet | 6.5 feet | 21.5 feet |
| Town Center Blvd | 15 feet | 6.5 feet | 21.5 feet |
| Shell Blvd | 15 feet | 6.5 feet | 21.5 feet |
| Foster City Blvd | 15 feet | 6.5 feet | 21.5 feet |
| State Route 92 | 15 feet | +31.0 feet | +46.0 feet |

(a) Area between face of curb of the roadway to the property line.

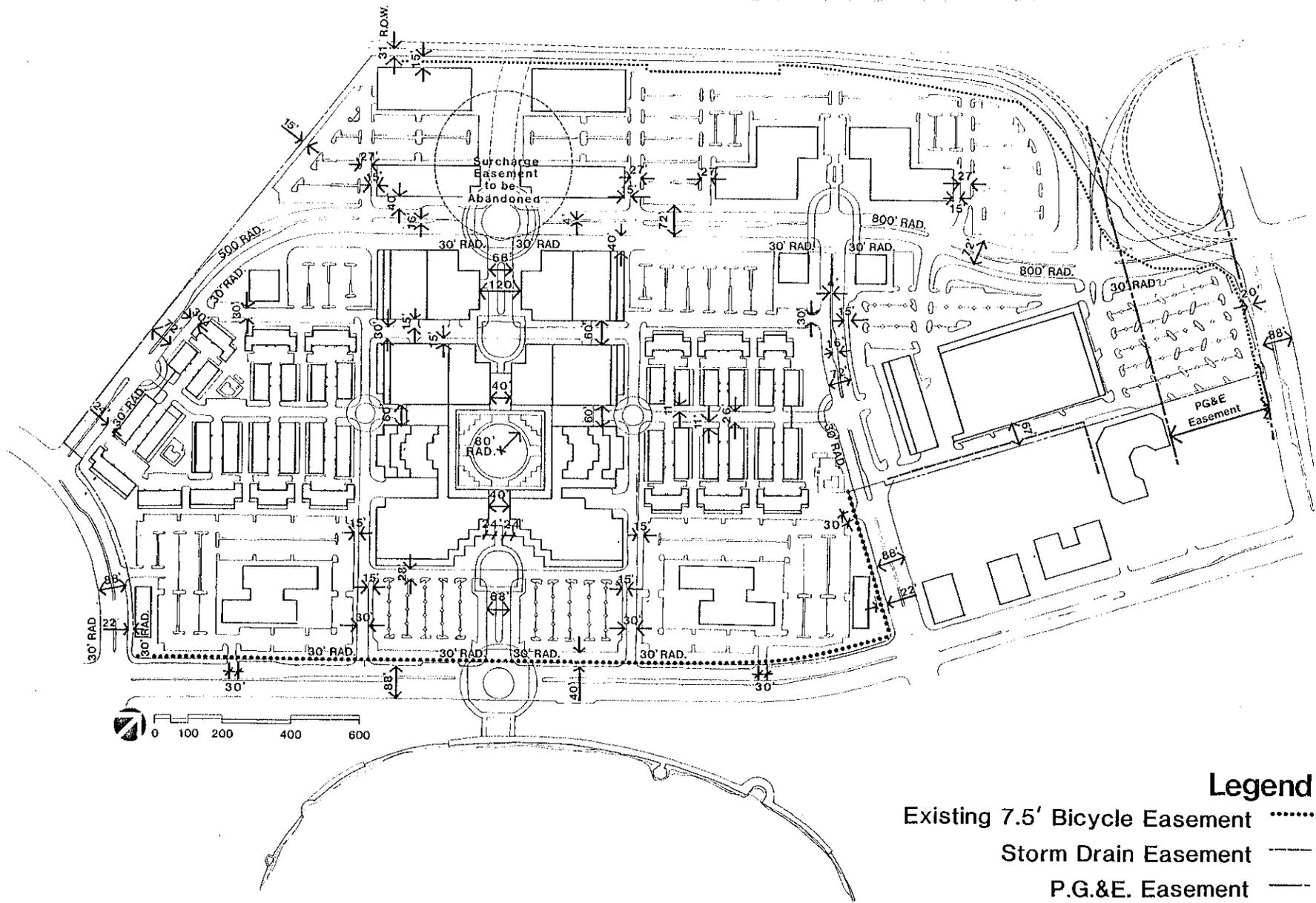
Building Setbacks

All buildings and parking areas in Town Center will be setback from the public roadways behind the Landscape Easement Lines. Moreover, all buildings facing Town Center Boulevard will be setback approximately 40 feet from the curb face of Town Center Boulevard. The Building Setback Table below presents the approximate building and/or parking setback dimensions from the curb face of the various streets.

BUILDING SETBACK TABLE

| <u>Street</u> | <u>Building Setback (a)</u> |
|---------------------|-----------------------------|
| East Hillsdale Blvd | 40.0 feet |
| Edgewater Blvd | 21.5 feet |
| Town Center Blvd | +40.0 feet |
| Shell Blvd | 21.5 feet |
| Foster City Blvd | 21.5 feet |
| State Route 92 | +46.0 feet |

(a) The dimension from face of curb of the roadway to the face of building or parking area.



- Legend**
- Existing 7.5' Bicycle Easement (dotted line)
 - Storm Drain Easement --- (long dashed line)
 - P.G.&E. Easement — (solid line)
 - Slope Easement (short dotted line)

Site Dimensions

Site Circulation

The Site Circulation Plans illustrate the various access points and routes of travel on the Town Center site.

Vehicular Circulation

Vehicular circulation is addressed in the accompanying plans. For purposes of this presentation, the site is divided into three zones: (1) the Town Center core area, (2) the area north of Town Center Boulevard, and (3) the regional retail site.

Town Center Core Area

This zone represents the largest area and greatest intensity of use for the project. Major access is through two north-south entry and exit drives which lead from Town Center Boulevard and East Hillsdale Boulevard and are aligned with the central axis of the plan. These access drives each consist of an entry and exit drive dimensionally 2-lanes wide and separated by a 20-foot median. Both drives are to be signaled allowing the safety of left turn ingress and egress.

Two additional major access points along East Hillsdale Boulevard allow entry to the central core area. These delineate internal drives along the eastern and western edges of the mid-rise core area. Together with the east/west internal drive along the northern edge of the mid-rise core area, these drives form a rectilinear perimeter circulation route internal to the site. Dimensionally, the internal drives are 2-lane roadways, approximately 30 feet in width. Key intersections on this perimeter circulation route are given added significance through special pavement and landscape treatment. Please refer to the Landscape discussion for a detailed description of these intersections.

Major access into the townhouse and mid-rise residential developments is provided via the northern entry and exit drive from Town Center Boulevard to the two internal north-south circulation drives. In addition, two secondary or minor residential access routes are provided from East Hillsdale Boulevard along those same internal drives. These access points will facilitate vehicular movement along the central core area into the residential developments.

The townhouse residential circulation is arranged on an east/west axis perpendicular to the main circulation axis of the plan. Special intersections mark this cross-axis on the internal circulation route.

The residential streets are to have controlled vehicular access at both these special intersections as well as on Town Center Boulevard for the west parcel, and Shell Boulevard for the east parcel. Within each parcel, the residential street forms the trunk of a regular series of branching drives to the individual townhouse units, producing an urban flavor to the development by its symmetry and grid-like arrangement. The residential street is to be approximately 26 feet in width.

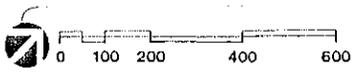
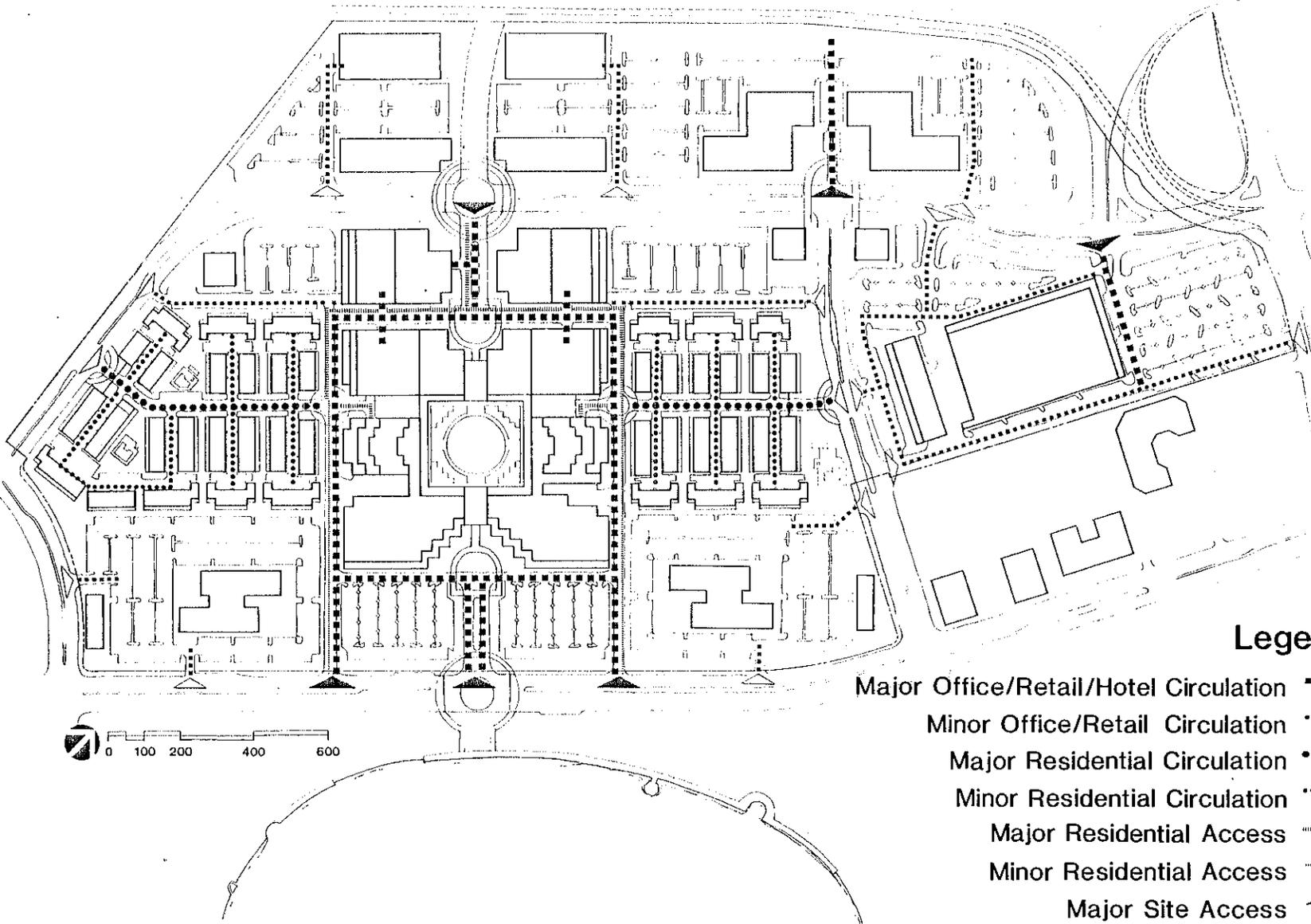
Access is provided to the structured parking of the mid-rise residential blocks on the core side of the special residential intersections.

Circulation for the office parcels along East Hillsdale Boulevard is independent from Town Center's internal circulation system. Each parcel has two minor access points. For the west parcel, access is from East Hillsdale Boulevard and from Edgewater Boulevard. For the east parcel, access is from East Hillsdale Boulevard and from Shell Boulevard. The access points from East Hillsdale Boulevard are to be spaced equally between the entries to the internal drives and the Edgewater and Shell Boulevard intersections and are to be provided with turn-in pockets. Two minor office/retail collector routes to the central core occur on an east-west axis. These routes pass through parking lots adjacent to Town Center Boulevard, linking with the rectilinear perimeter internal circulation route. This circulation route also provides an added route for ingress and egress to the structured parking associated with the hotel, six-story office structure and mid-rise office structures. Entry to these structures is on the internal circulation route along the northern edge of the mid-rise core area.

Area North Of Town Center Boulevard

The second major circulation zone is the area north of Town Center Boulevard. This area is independent from the internal circulation system of the central core area, although connected via Town Center Boulevard and the north entry drive.

Four access points are provided from Town Center Boulevard to provide for ease of parking and direct access to the office buildings. The major access point will be at the intersection of Shell Boulevard and Town Center Boulevard bisecting the two three-story office buildings. Three other minor access points will insure efficient and well distributed circulation to the surface parking areas and the two parking structures.



Legend

- Major Office/Retail/Hotel Circulation
- Minor Office/Retail Circulation
- Major Residential Circulation
- Minor Residential Circulation
- Major Residential Access
- Minor Residential Access
- Major Site Access
- Minor Site Access

Vehicular Circulation

Regional Retail Site

The third major circulation zone is the regional retail site. Five access points are provided for this area. The major access point is from Town Center Boulevard directly opposite the State Route 92 on-and-off ramps. Other minor access points allow traffic to enter and exit from Shell Boulevard, Foster City Boulevard and Town Center Boulevard. Access from Town Center Boulevard will allow left turn movements to-and-from the site. Access to the site from Foster City Boulevard will be provided with a turn-in pocket. The layout of this site with several access points will allow for convenient circulation.

Pedestrian/Bicycle Circulation

A second level of site circulation is illustrated in the accompanying plan: Pedestrian/Bicycle Circulation. The plan depicts the hierarchy of pedestrian circulation routes, and identifies the on-site bike path which is to connect to existing routes within Foster City.

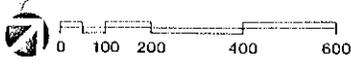
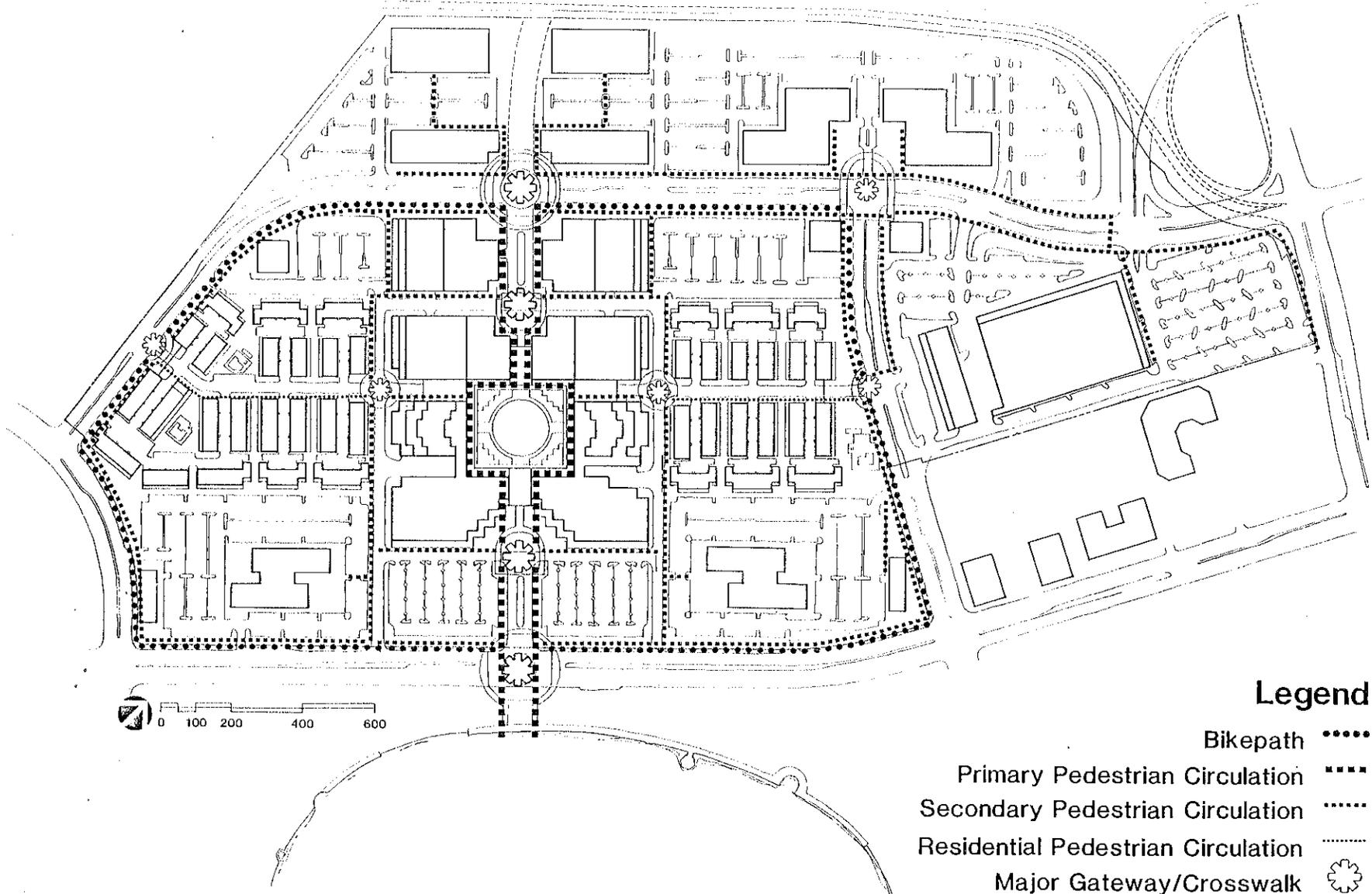
Primary pedestrian circulation is identified along the north-south axis of the central open space corridor of Town Center from the freeway overpass at its junction with Town Center Boulevard on the north, to Leo J. Ryan Park on the south. Special intersections and crossings provide this pedestrian route with places of interest along its length. The central landscaped open space and its surrounding arcade provide the focal point of this pedestrian circulation pathway, in keeping with the focus and greater level of activity which is to represent the center core of Town Center.

Secondary pedestrian pathways occur along the townhouse residential side of the internal north-south drives of Town Center, at various east-west axes through the site and around the perimeter of the development bounded by East Hillsdale, Edgewater, Town Center and Shell Boulevards.

The residential pedestrian pathway represents a major east-west route for on-site residents to access the central open space area and its activities. This strong pedestrian link also ties the townhouse residential community to the retail along East Hillsdale as well as off-site to Leo J. Ryan Park.

To the north of Town Center Boulevard, the office buildings are tied together with pedestrian walks along Town Center Boulevard.

The regional retail site is accessed with pedestrian walks along Town Center, Shell and Foster City Boulevards. These walks provide the east-west connection with the Town Center central core area along Town Center Boulevard. Residents of the townhouse community are given the added advantage of access along the residential street directly into the regional retail site at a Shell Boulevard crossing.



Legend

- Bikepath (dotted line)
- Primary Pedestrian Circulation - - - - (dashed line)
- Secondary Pedestrian Circulation - . - . - . (dash-dot line)
- Residential Pedestrian Circulation (dotted line)
- Major Gateway/Crosswalk * (starburst symbol)
- Minor Gateway/Crosswalk * (smaller starburst symbol)

Pedestrian/Bicycle Circulation

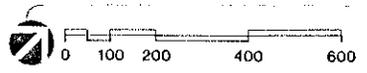
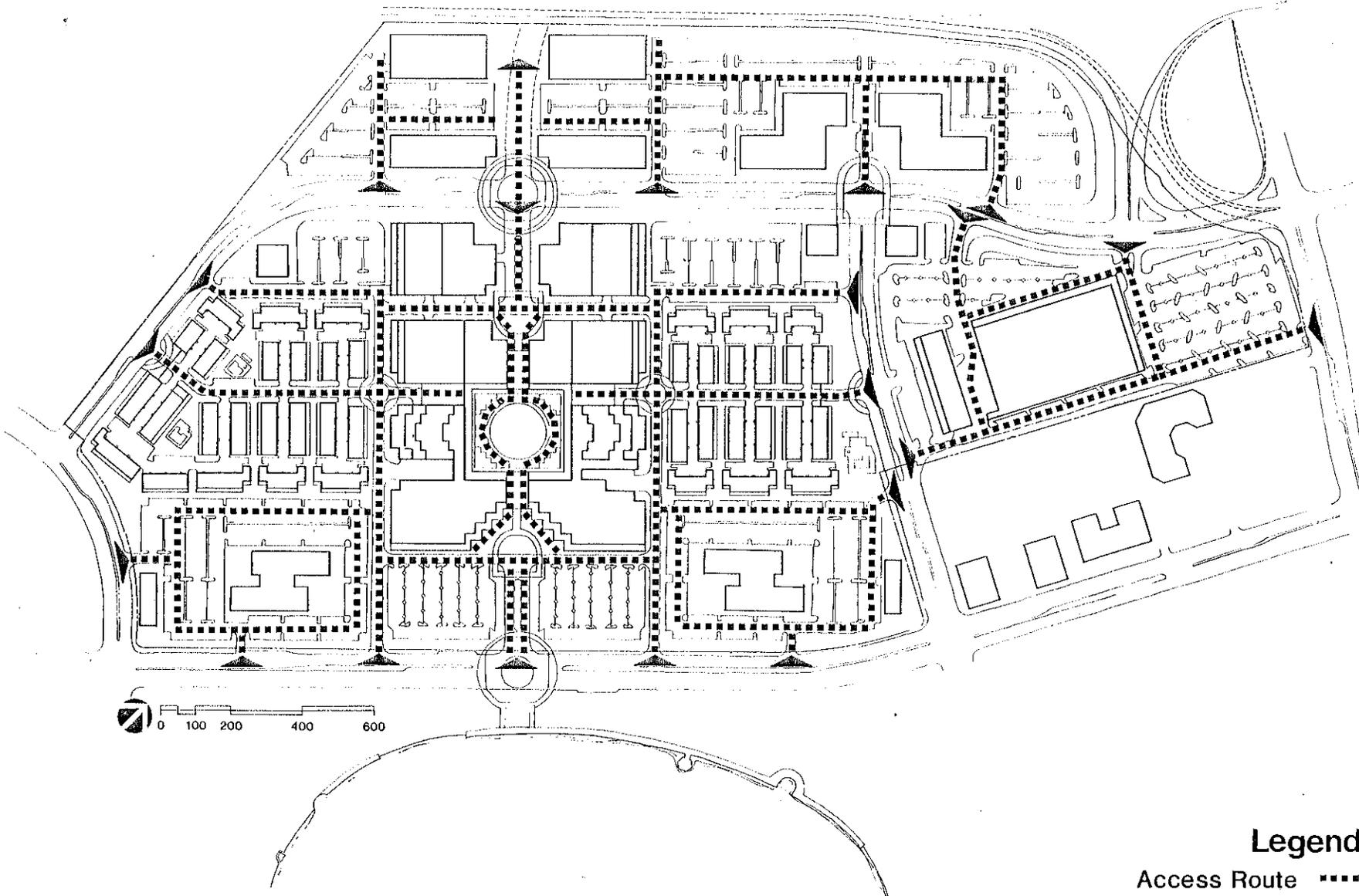
Emergency/Fire Circulation

Town Center is to be connected to the general street system of Foster City at several points allowing ready access by fire and emergency vehicles. All entry points and drives within the internal circulation system of Town Center are to be designed to meet fire and emergency vehicle driving and turning requirements. Please refer to the Site Dimensions section for the technical information related to these various entry points and drives on the site.

Fire apparatus access will be provided to all buildings within the project. Access will be constructed prior to start-up of each development phase. This may include the construction of temporary roads to reach buildings adjacent to undeveloped portions of the site.

Emergency circulation for the central open space area is to be addressed by providing access at several points. The north and south portal entries are to be designed for fire truck access, while the cross-axis pedestrian entries can be used to place an emergency vehicle within easy reach of many parts of the central open space and surrounding retail. Final central open space access provisions will be closely coordinated with the Fire Department.

In addition to the public roadway access points, emergency access if needed, to the office parcels along East Hillsdale Boulevard will be provided by special fire and emergency lanes through the landscape reserve, along the internal drives. Final provisions will be closely coordinated with the Fire Department.



- Legend**
- Access Route - - - -
 - Access Point ▲

Emergency & Fire Circulation

Site Landscape Concept

Town Center can be divided into several zones which are characterized by different landscape treatments. These divisions include the central landscaped open spaces/Pedestrian Promenade, building plazas, residential commons, rooftops, streetscapes, parking areas, and buffers. Each zone addresses site-specific solutions relating to usage, views, image, screening, linkages, and microclimatic conditions. Together these solutions reinforce a strong, clear, and unified landscape concept.

Central Promenade

The pedestrian promenade linking East Hillside and Town Center Boulevards is one of the most important features of the Town Center development. This central open space corridor is to be reinforced, where possible, with double rows of broadleaf evergreen trees, thereby providing a strong legible form throughout the year. These rows of trees also frame and focus the views through this corridor. Within the middle portion of the central open space, tall accent trees provide a visual focus and windbreak, while smaller deciduous shade trees provide comfortable, intimate seating areas at the edges. These smaller deciduous trees should be open and airy and provide fall color, winter sun and spring interest.

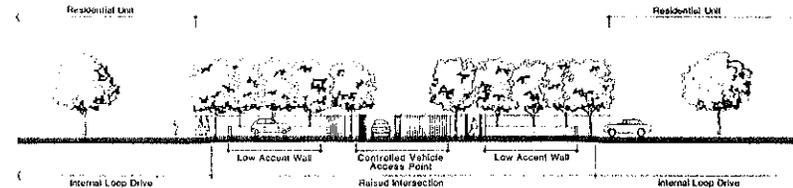
Streetscapes

The boulevards and roadways of Town Center are to be developed as formal planted corridors. Broadleaf evergreen trees used along these roadways will help to define the vehicular circulation throughout the year. Smaller scale accent plantings are to be included in the boulevard medians for added interest. Prototypical streetscape treatments are included in the Prototypical Treatments Section of the Design Guidelines and are keyed to this Site Landscape Concept Plan.

Gateways

The Gateway category relates to the special intersections described earlier in this section. At the major gateways of the north and south entry drives from East Hillside Boulevard and Town Center Boulevard, small deciduous flowering accent tree plantings combine with a sea of flowering groundcover and small shrubs to draw and focus attention. The minor gateways along the Central Promenade utilize this same planting theme.

The residential gateways as well as the gateway at the Town Center/Shell Boulevard intersection are to be less dramatic than the Central Promenade gateways in terms of their planting schemes. Broadleaf evergreen trees with special character are to be used instead of flowering deciduous trees and areas for flowering groundcover or shrubs are to be reduced.



Section C - Residential Gateway

Residential Commons

Landscaped areas within the townhouse residential parcels relate to the smaller scale and residential character of these developments. A mixture of plant types is recommended with care given to texture, color and form at a residential scale. The residential street through the parcels provides the formal "backbone" of planting, reinforcing the geometry of the branching circulation system and tying these developments with the rest of Town Center.

Buffers

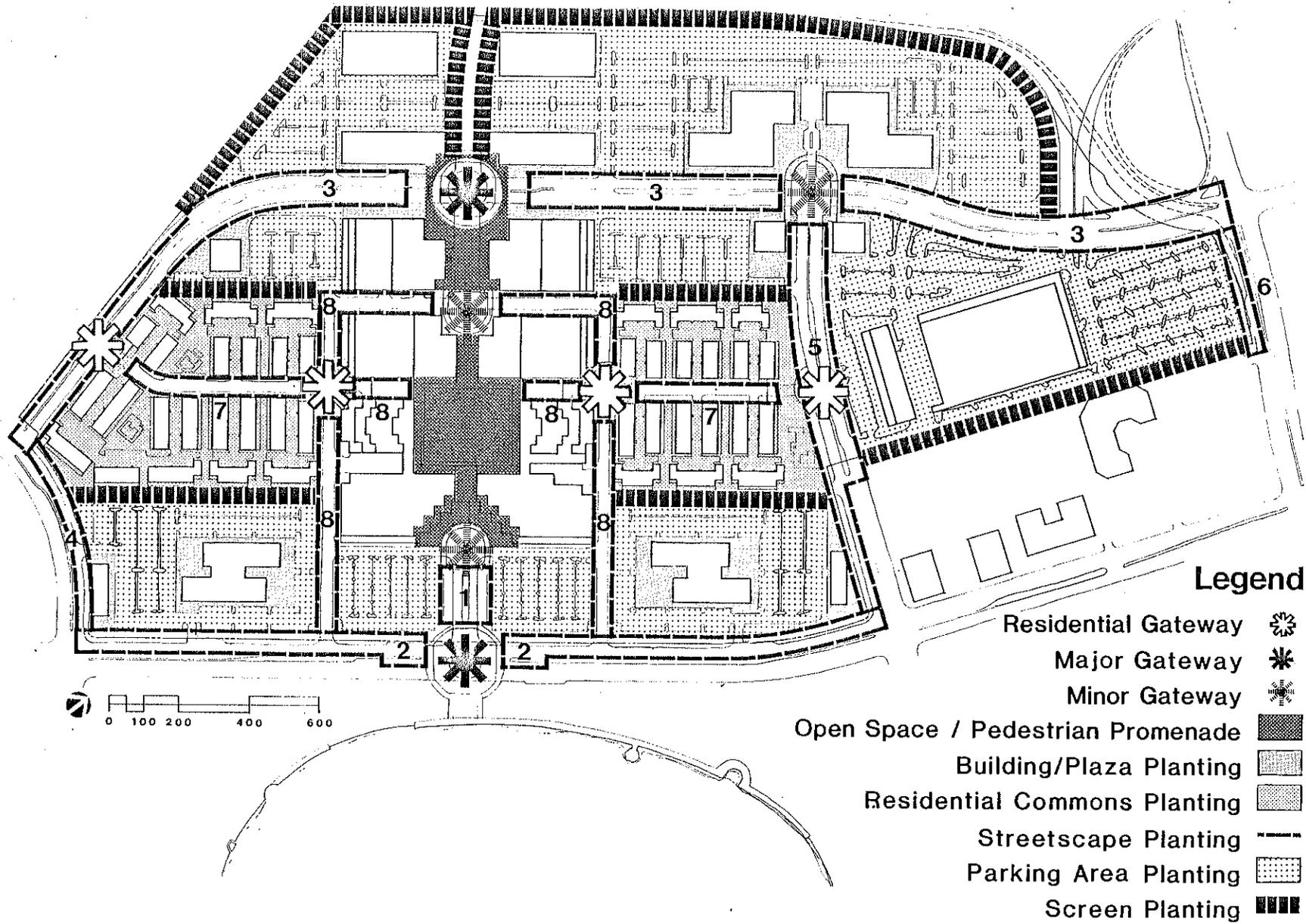
Where visual buffering is desirable, dense screen plantings are indicated. These consist of broadleaf evergreen or coniferous trees and evergreen hedges. Specific buffer treatments are illustrated for typical conditions in Section 3, Design Guidelines.

Building Plazas

Special identity and character should be provided in these areas using broadleaf evergreen and accent trees, and planting beds or pots. Plantings should be kept in simple masses of color and textural compositions and should tie into streetscape plantings.

Parking Areas

Surface parking areas are treated with tree wells and medians as detailed in Section 3. Large deciduous shade trees and median hedge plantings help screen and soften these paved areas. The deciduous shade trees provide seasonal color and interest, contrasting with the evergreen character of the street plantings.



Legend

Residential Gateway 

Major Gateway 

Minor Gateway 

Open Space / Pedestrian Promenade 

Building/Plaza Planting 

Residential Commons Planting 

Streetscape Planting 

Parking Area Planting 

Screen Planting 

Site Landscape Concept

Site Signage Concept

Major Site Identification

The intent of the major external vehicular and pedestrian signage is the development of a system which is informational and compatible with the total project design concept and adequately identifies the commercial aspects. A unique, consistent theme is desired for Town Center which serves to unite all sign types into a recognizable system drawing the visitors into the central core of the project. Commercial users will need vehicular identity, which can include the utilization of a reader board system of signage.

Primary Entrance

- identifies primary project entrance at East Hillsdale Boulevard with project name and address
- a sign face which may be illuminated and approved by the Town Center architectural review committee
- to be architectural in nature establishing a formal entrance to the project
- placed in landscaped beds at the primary intersection and oriented for maximum visibility along the vehicular corridor
- durable vandal-resistant construction, colors and materials to complement architectural site work.

Secondary Entrances

- identifies secondary project entrances with name and address
- a sign face which may be illuminated and approved by the Town Center architectural review committee
- two individual single-faced signs or one double-faced sign at each entrance
- to be placed in landscaped beds at secondary intersections and oriented for maximum visibility along vehicular corridors
- durable, vandal-resistant construction, colors and materials to complement architectural site work.

Internal Signage

Internal Signage is used to insure a controlled vehicular and pedestrian traffic flow within the project. A unified system helps to identify project

boundaries and permits unassisted access to all parking and entry points.

Directional Signage

- directs vehicular traffic to all facilities within the project
- modular, stacking sign units can be quickly revised to meet expanding needs of the phased development
- placed in landscaping along traffic corridors and at intersections
- pedestrian scale directional units located at internal walkway intersections and plazas
- durable, vandal-resistant construction with highly legible typography

Reassurance Signage

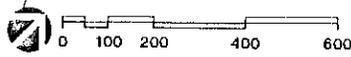
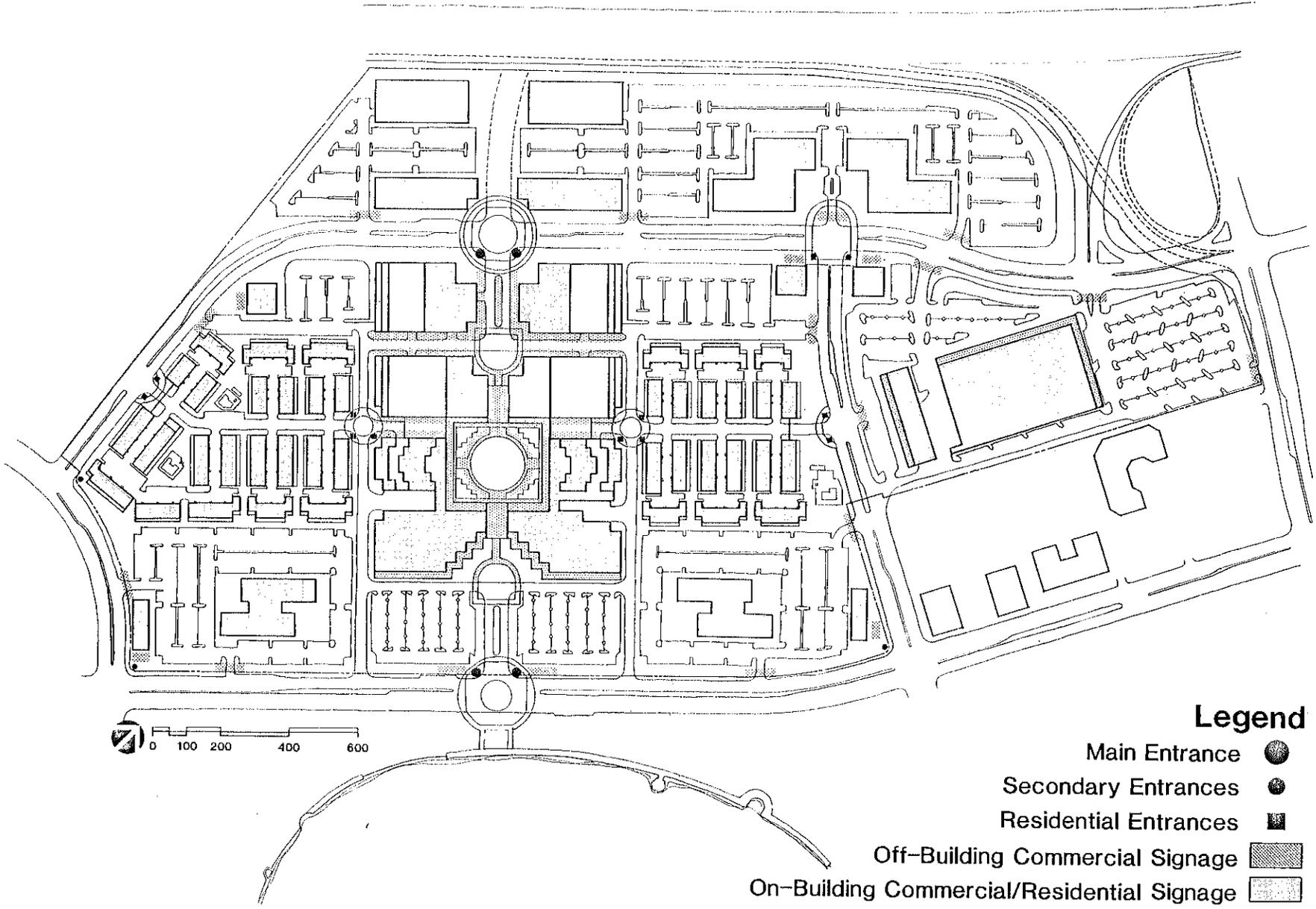
- reinforces directional signage by identifying secondary entrances, parking, delivery, etc.
- modular, stacking, reduced scale directional sign units
- placed in landscaping at intersections and traffic entrances
- durable vandal-resistant construction with highly legible typography.

Functional Signage

- functional system of regulatory and prohibitive signage for internal traffic control
- international symbols developed by the Department of Transportation to be used when possible (Bus Stop, No Parking, etc.)
- durable, vandal-resistant post and panel signs with highly legible symbols and typography

Interim Signage

The phasing of the Town Center Project presents a need for extensive interim signage identifying new construction and tenants. Establishment of design guidelines for interim signage insures uniformity throughout the phasing and provides a format linking the Town Center identity with the interim information.



Legend

- Main Entrance ●
- Secondary Entrances ●
- Residential Entrances ■
- Off-Building Commercial Signage [diagonal lines]
- On-Building Commercial/Residential Signage [stippled]
- Commercial Signage/Banners/Flags [horizontal lines]

Site Signage Concept

- carries Town Center identity and colors and identifies building information and marketing information, 800-square foot maximum sign face
- carries Town Center identity and colors and identifies developer, architect, tenants, contractors, financing institutions, and marketing information on a 100-square foot maximum sign face
- design and fabrication according to specific guidelines provided by an architectural review committee
- freestanding, post and panel signage with painted graphics

Commercial Signage

The intent of the commercial signage is to adequately identify individual businesses, offices, and residential complexes while establishing an urban character to the development. Where appropriate, commercial signage is to be visible from the auto. Signage is recommended which promotes a "town-like" feeling and offers a variety of visual images especially within the central commercial core of Town Center. The commercial core signage system includes an identification system along the major pedestrian entry plaza areas. Tenant signage may not project above the architectural profile of the building behind such signage. All commercial users will need vehicular identity, which can include the utilization of a reader board system of signage.

Off-Building Signage

- identifies individual or multiple tenants and applies to residential, commercial and office tenants
- located at frontages facing public vehicular and pedestrian corridors
- internal illumination, maximum sign face footage determined by primary business frontage. The sign area allowed for the Primary Business Frontage identification sign shall be one-half (1/2) square foot of sign area per one (1) linear foot of business frontage or twenty (20) square feet, whichever is greater as stipulated in the Foster City zoning regulations.
- to be placed in landscaped areas along primary frontage and oriented for maximum visibility
- durable, vandal-resistant construction, colors and materials to complement architectural site work.

On-Building Signage

- identifies individual or multiple tenants and applies to residential, commercial and office tenants
- located on canopy fascia, beneath canopy overhang, or directly on building face at main entrance
- may be applied as individual letterforms or symbols applied directly to the building face
- internal illumination, maximum sign face square footage as determined by primary frontage. The primary frontage for On-Building Signage shall not exceed fifty (50) square feet or fifteen (15) percent of the building face, whichever is greater as stipulated in the Foster City zoning regulations.
- to be provided by individual tenants. Design and fabrication of signage done under guidance and approval of an architectural review committee.

Banners, Flags and Pennants

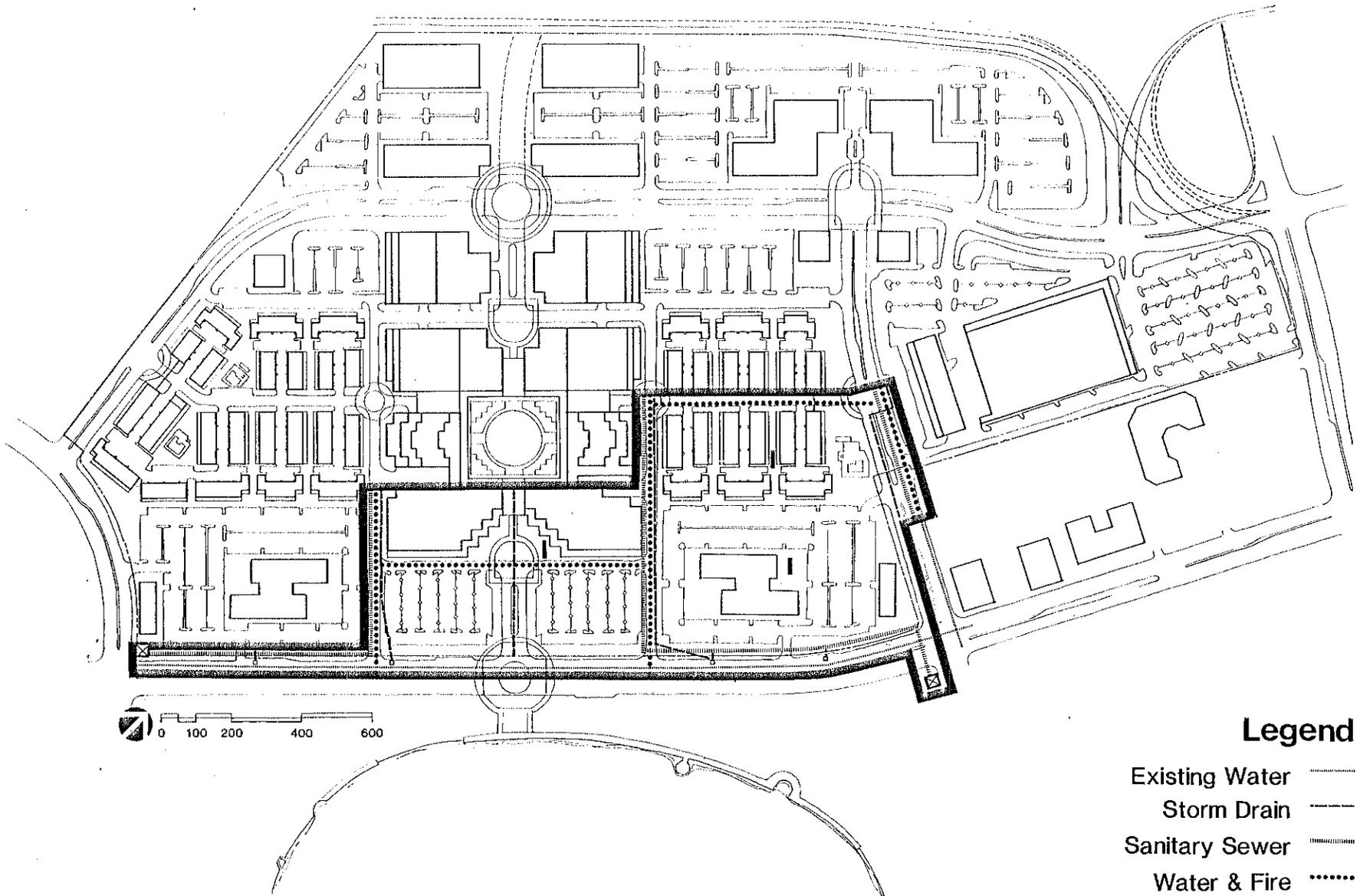
- may be located in pedestrian walkways under canopies, hanging from overhead structures and overhangs, flying from poles and custom-built frameworks.

Phasing and Implementation

The discussion on the phasing and implementation of the Town Center project has been divided into three parts: Site Utilities Phasing Plans; a Public Development Phasing Plan, and a Private Development Phasing Plan.

Site Utilities

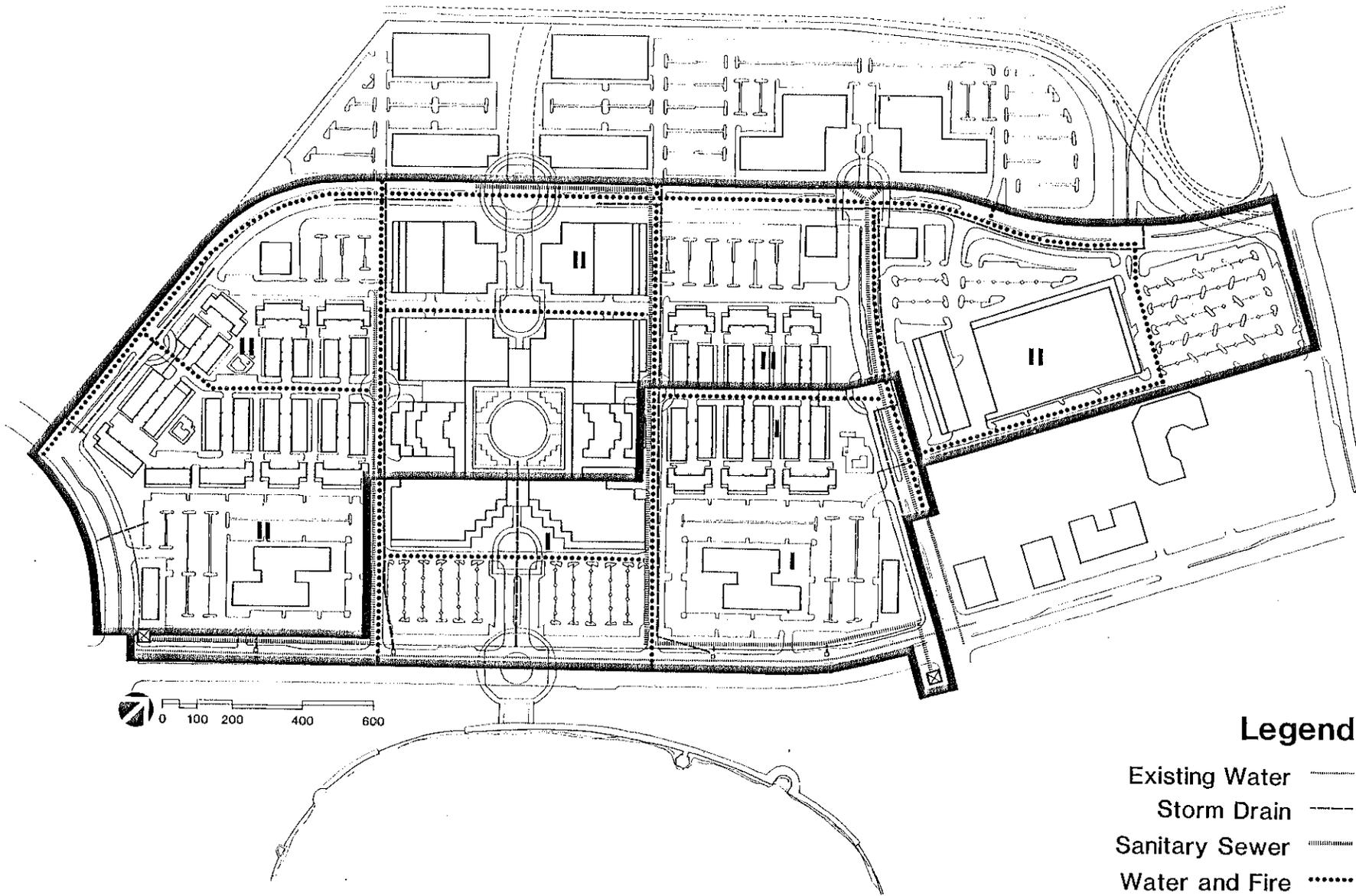
Full utilities, as depicted on the three site utility phasing plans, will be provided to serve each phase of the private development: Phase I - 1984/1985; Phase II - 1985/1986; Phase III - 1986/1987. Specifically, each phase will be provided with positive drainage for storm water runoff, sewer mains, required sewerage pump station capacity and domestic water service. Looped water mains will be provided for each phase meeting the fire flow requirements of the Foster City Fire Department. The phasing of the fire water system will be such that adequate protection will be provided during the construction of buildings and other combustible facilities.



Legend

- Existing Water ————
- Storm Drain ————
- Sanitary Sewer ————
- Water & Fire ······
- Pump Station ☒

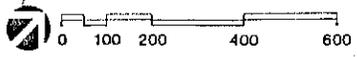
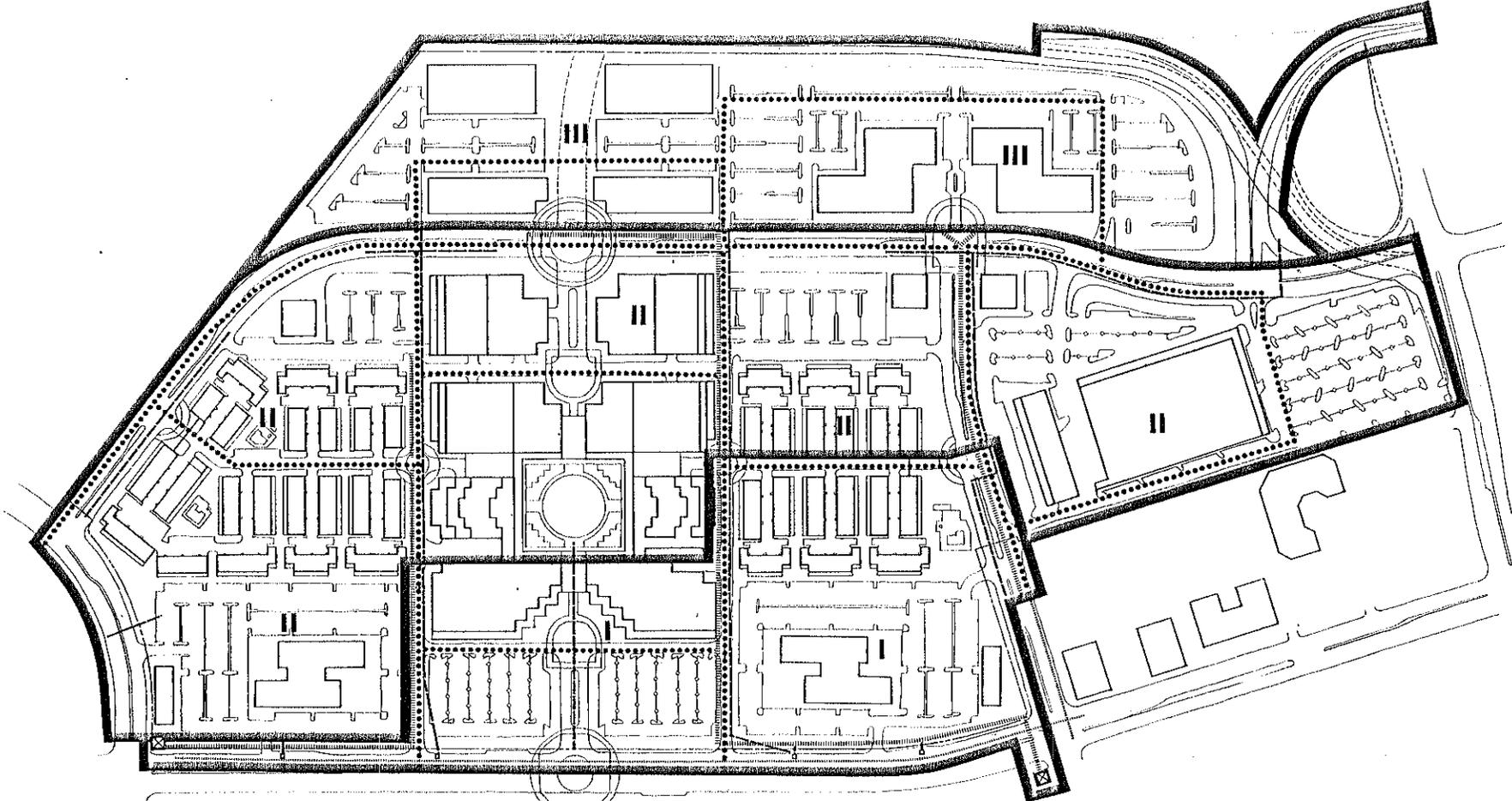
Site Utilities Phase I



Legend

- Existing Water ———
- Storm Drain - - - - -
- Sanitary Sewer
- Water and Fire
- Pump Station ☒

Site Utilities Phase I & II



Legend

- Existing Water ————
- Storm Drain - - - - -
- Sanitary Sewer ······
- Water & Fire ······
- Pump Station ☒

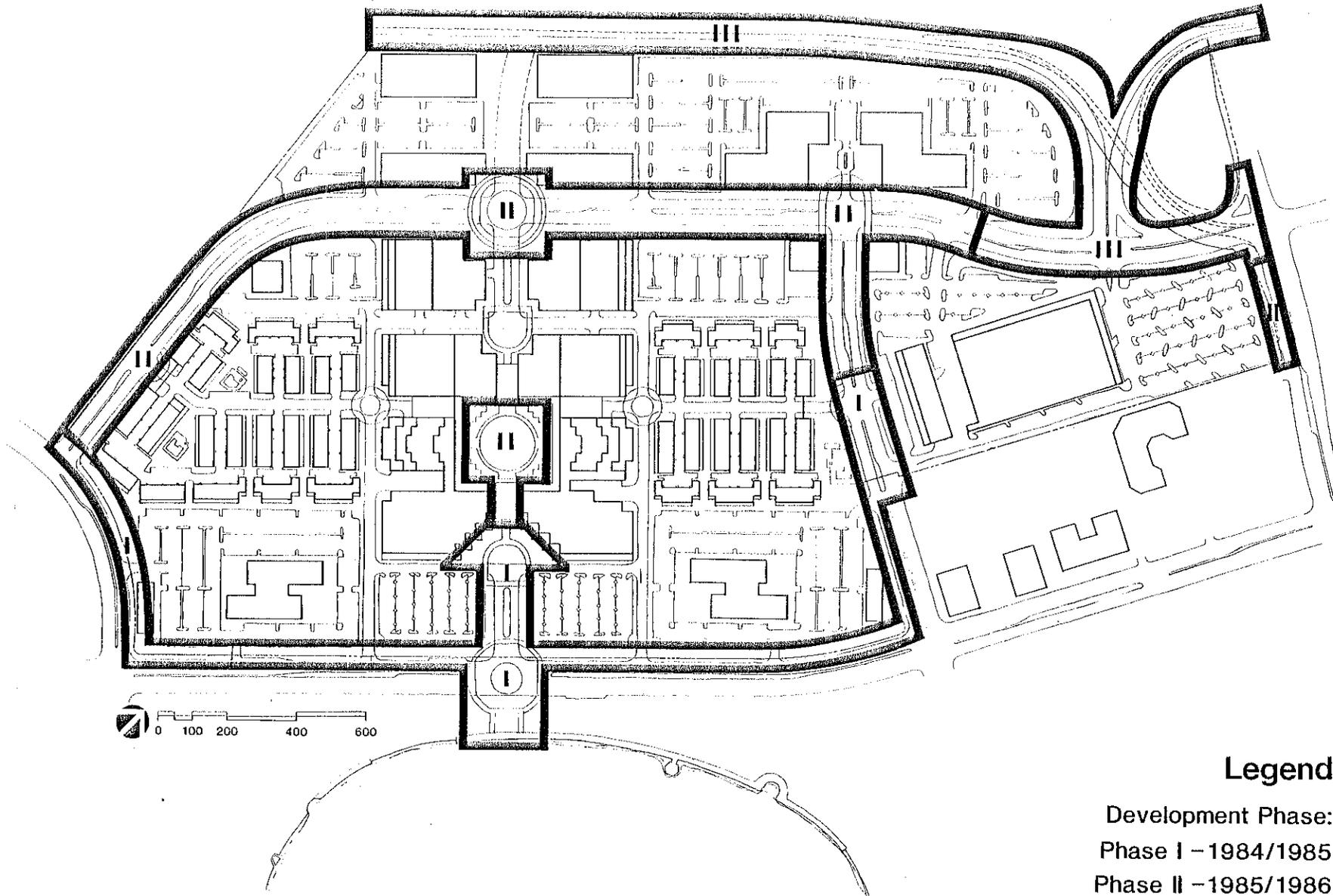
Site Utilities Phase I, II & III

Public Development

A phasing plan included in this section illustrates the Public Development portion of the project. Areas designated public on this plan include perimeter streets and landscaping, utilities within the rights-of-way, and open space areas of the Central Promenade including the Town Center park. The phasing for the Public Development portion of the project is scheduled to occur over an approximate period of three to four years as private development warrants. Monies for the public development may be obtained through the establishment of an Assessment District.

Phase I of the Public Development includes the landscape easements for Edgewater Boulevard and East Hillside Boulevard, the central portion of the Shell Boulevard right-of-way and its landscape easement and utilities, and the major entry to the central open space corridor from East Hillside Boulevard. These portions of the Public Development Plan occur concurrently with the Phase I portion of Private Development. Phase II of Public Development constructs Town Center Boulevard and its landscape easements and utilities, the northern portion of Shell Boulevard, and the Town Center park. The portions of the circulation network developed in this phase will serve the remaining five private development phases. The construction of a landscape easement along Foster City Boulevard is scheduled for commencement upon development of the regional retail site but no later than in Phase III.

The third and final phase of public development involves the redesign and reconstruction of the east-bound on-and-off ramps of State Route 92 at Foster City Boulevard. This will complete the development of Town Center Boulevard into the State Route 92 interchange and through to Foster City Boulevard. This work is to be coordinated and designed with the California Department of Transportation. The commencement of construction of this final phase of public improvements is scheduled to occur the earlier part of 1987 or upon receipt of building permits representing a cumulative total to date of 700,000 square feet of office buildings.



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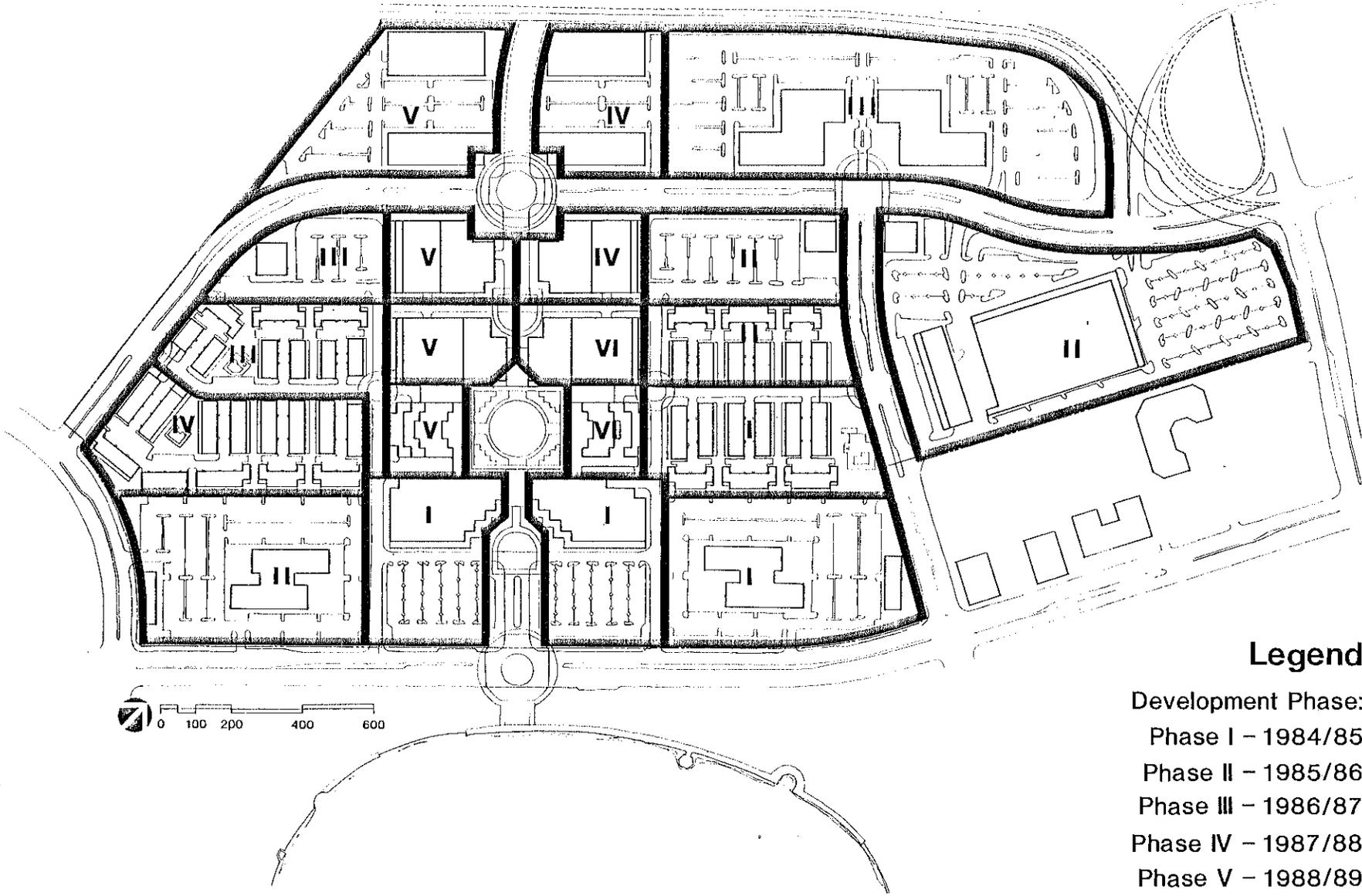
- Development Phase:
- Phase I - 1984/1985
- Phase II - 1985/1986
- Phase III - 1986/1987

Public Development Phasing

Private Development

The third portion of the phasing and implementation discussion contains information concerning the anticipated phasing of Private Development. All structures, parking, and interior streets and their landscaping are included in the implementation schedule for the project which is projected for a seven year period.

In the first phase of Private Development, the southeast office site on East Hillsdale Boulevard, the southern portion of the eastern townhouse residential site, and the large retail sites fronting East Hillsdale are envisioned for development. Phase II will involve the remaining portion of the eastern townhouse residential site, the southwestern office site on East Hillsdale Boulevard, the regional retail site, and the retail site at the southwest corner of Town Center Boulevard and Shell Boulevard. The easternmost office structures north of Town Center Boulevard will comprise Phase III, along with the northern portion of the western townhouse residential site and the westernmost retail site facing Town Center Boulevard. Phase IV consists of the two office projects on the eastern side of the intersection of the major north entry drive from Town Center Boulevard as well as the southern portion of the western townhouse residential site. Phase V includes the hotel, the mid-rise office in the northernmost corner of the project site, the western mid-rise office tower just south of the hotel, and the western mid-rise residential element. The final private development phase consists of the remaining eastern mid-rise office tower and mid-rise residential element.



Legend

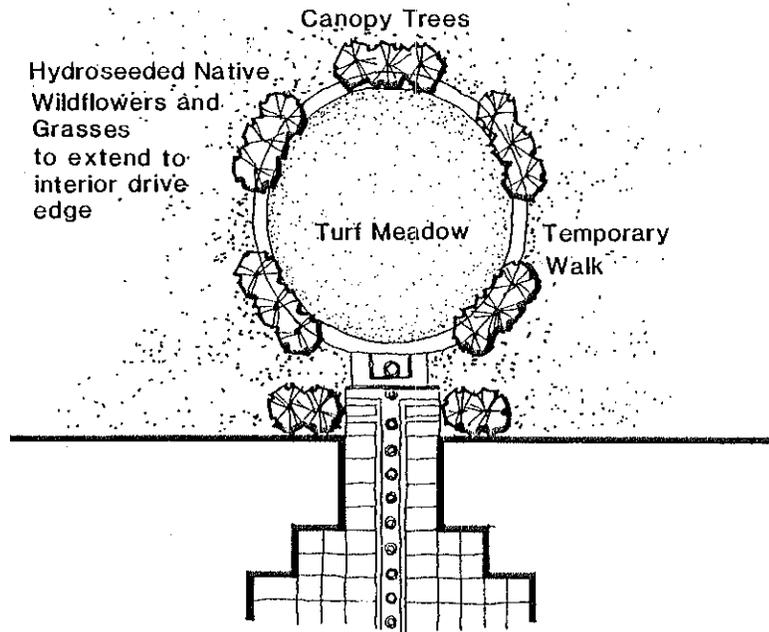
- Development Phase:
- Phase I – 1984/85
 - Phase II – 1985/86
 - Phase III – 1986/87
 - Phase IV – 1987/88
 - Phase V – 1988/89
 - Phase VI – 1989/90

Private Development Phasing

Interim Treatments

Due to the time periods between various phases of development, interim treatments may be necessary for different portions of the site. These treatments are to give an attractive appearance to the developments and to mitigate the effects of construction in later phases.

For the early phases of the townhouse development, temporary roads may be necessary to provide access for emergency vehicles. It will also be necessary to provide an interim landscape treatment for the central landscaped open space area between its development and the Phase V and VI mid-rise developments. Interim planting is to be developed around the central open space area and will consist of turf throughout the area itself with hydroseeded native grasses and wildflowers for the mid-rise residential sites which flank the area. This interim landscape treatment will give an attractive appearance to this central open space while allowing it to function properly prior to the construction of the sites surrounding it.



Interim Treatment - Central Open Space

Design Guidelines

Overview

To implement Town Center parameters are required which will guide development and provide a continuity to the project as it progresses over time. These parameters are embodied in the following Design Guidelines which address the methods, concepts, elements and materials which should be adhered to by the various parties who become involved in Town Center development through the years. The Guidelines provide the framework for which design and implementation should occur in order to assure the harmony and integrity of each project element in the context of the entire development. Detailed design work is to be resolved as each individual element of Town Center undergoes precise study for its site use permit and subsequent implementation.

This section continues with four parts: (1) Architectural Guidelines (2) Landscape Guidelines (3) Parking Guidelines and (4) Signage Guidelines.

Architectural Guidelines

The Architectural Guidelines set the parameters for building implementation at Town Center. Included in this section are discussions on Building Siting, Heights and Massing, Scale and Character, Materials and Colors, Rooftop Treatments and Service and Delivery Areas.

Siting

The careful siting of Town Center buildings within their individual parcels is important for maintaining views, spatial definition and the "town center" image of the overall project. All buildings within Town Center should adhere to the overall master planning "grid" described in the second section - General Plan Description.

The proper siting of buildings in the Town Center core area is important because of the close integration of buildings with open spaces. Structures in this area should extend to the rights-of-way for each parcel. This alignment reinforces the urban character of the streetscapes and defines the pedestrian and view corridors of the north/south major axis and the east/west minor axis.

Buildings along both sides of Town Center Boulevard should extend to the planning "grid" defined by the 40-foot building setback. This arrangement provides an organization to Town Center Boulevard consistent with the "Town Center" theme of the project by positioning buildings along a uniform line.

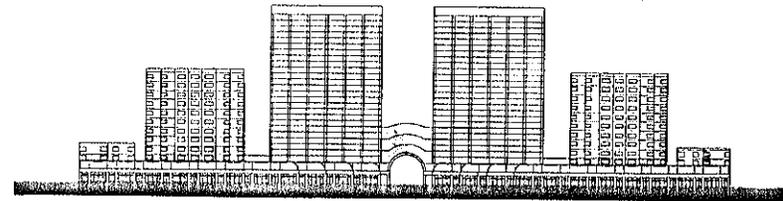
Siting of buildings north of Town Center Boulevard opposite the central core should follow the site lines established by the central core buildings. The positioning of these two office buildings and parking structures should extend to established site lines.

The retail buildings and portions of the office buildings at the intersection of Shell Boulevard and Town Center Boulevard should respond to both the east/west planning grid along Town Center Boulevard and the north/south grid along Shell Boulevard. Siting of these buildings along the planning "grids" also gives these buildings an organization which positively responds to the rest of the Town Center development.

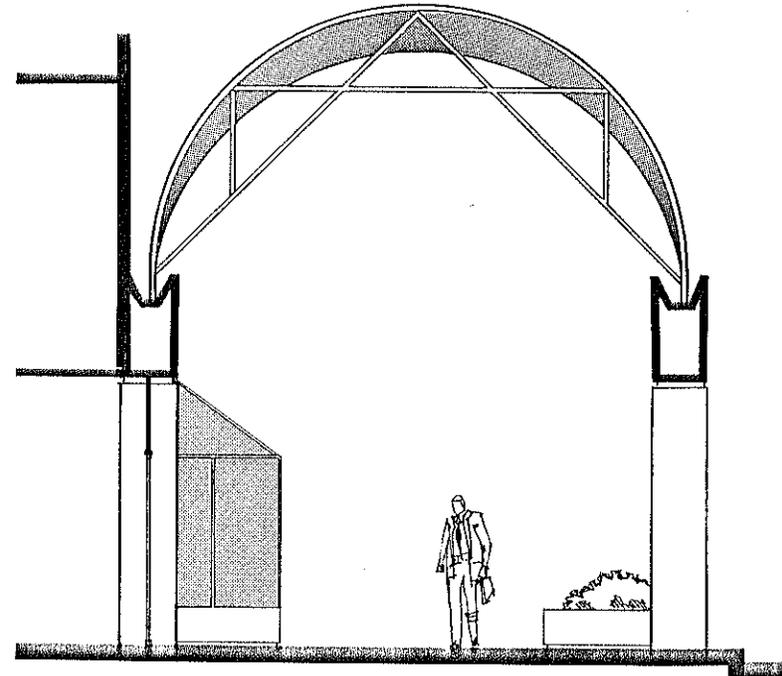
Scale and Character

The scale and character of architecture in the Town Center development should impart a sense of "town center" as well as develop a pedestrian nature to the project. Building massing, configuration, density, facade treatments, and detailing should all reinforce the urban scale of the project.

The canopied arcade running the length of the retail along East Hillsdale Boulevard and within the central open space perimeter expresses the character of these spaces and of the project. The arcade support structure should be the equivalent of one-story in height and of sufficient scale and proportion to define the space between the canopy edge and that of the storefronts behind it. The canopy should be a vaulted form and may be constructed of a translucent material admitting light while providing shade and weather protection. Graphic identification, banners, lighting, planting, seating, storefronts and paving must all be considered as integral to the arcade design. This design should reinforce the interactive urban and pedestrian market concept for the retail and central open space areas.



Arcade Elevation



Arcade Section

Materials and Colors

In order to be compatible with the urban theme of Town Center, the dominant materials used in the development should be stone, concrete, or masonry-like in nature. These materials will make the desired statement of an

urban center while contrasting well with the delicacy of the canopied arcade.

The colors of the project should appear light from a distance, allowing the building forms and detail to be defined by shades and shadows of natural light on planar surfaces. Details such as graphics, artwork, awnings, railings and trim can provide an accent of richer colors to contrast with the more subdued background color of the architecture.

Height and Massing

The buildings which make up Town Center should increase in height from lower buildings at the periphery of the site, to approximately twelve stories at the two central towers of the central core. It is also recommended that the mass of individual buildings be configured to contribute to the overall site massing, symmetry and visual movement towards the center of the project.

In addition to orienting the vertical mass of buildings on the site about the central core, building footprints should reinforce the sequence of spaces along the Central Promenade and along the major pedestrian arterials linking the site. Ground floor plans of individual buildings can be configured to enhance these spaces.

Massing of individual buildings should be used to delineate entrances and define circulation ways. Specific areas of concern are building edges that face onto plazas, intersections and pedestrian ways. Lines of site should be directed to these spaces through careful building massing.

Building massing should also be used to provide a transition to adjoining open spaces and adjacent structures. The mass of the structure should respond to adjacent smaller buildings or open space areas, thereby providing the transition necessary to effectively tie together the various uses.

Rooftop Treatments

Rooftop mechanical equipment and the upper decks of parking structures should be treated in a sensitive manner that addresses view conditions. The forms and materials used should be compatible with the rest of the architecture of Town Center. These treatments might include a trellis for portions of the parking structures or other architectural treatments which are to be sensitive to views.

Service and Delivery Areas

Service and delivery areas containing delivery docks, trash receptacles and bulk storage are to be shielded from the view of adjacent properties and the general public. Architectural screening should be compatible with the materials and forms of the buildings served. Where appropriate, the architectural screening should incorporate landscape planting to soften and additionally screen these areas from view.

Landscape Guidelines

The following design guidelines have been developed to provide a framework for future public and private landscape development within Town Center. These landscape guidelines address planting, paving, site furnishings, lighting, irrigation and erosion control. The guidelines promote the concept that a family of related materials should be implemented tying together the various buildings and open spaces, and enhancing the urban character of the development.

Plant Materials

The Town Center site has a high groundwater table while winds from the north-west contribute to the difficult growing conditions for plant materials. Selection of plant materials and their installation must be sensitive to these constraints.

The landscape plant materials should not only be for visual enjoyment, but should serve various functions within the urban environment. First, plantings should articulate outdoor spaces and define scale. For example, long rows of formal tree plantings focus views and define circulation corridors; formal bosques of trees can define a more intimate scale for seating by providing an overhead enclosure. In addition, plantings can be used for visual screening, provide for privacy and separation, or supplement architectural barriers.

Furthermore, plant materials should be arranged to provide shade and windbreaks where appropriate, and to reduce solar glare and heat build-up on parking areas.

Please refer to the Site Landscape Concept in the General Plan Description Section which defines the general types and uses of plant materials for the Town Center site.

Suggested Plant Materials List

| <u>BOTANICAL NAME</u> | <u>COMMON NAME</u> |
|--------------------------------------|----------------------------|
| STREET TREES | |
| Deciduous | |
| Platanus acerifolia | London Plane Tree |
| Platanus racemosa | California Sycamore |
| Alnus cordata | Italian Alder |
| Fraxinus uhdei | Evergreen Ash |
| Liquidambar styraciflua | American Sweet Gum |
| Ulmus pumila | Siberian Elm |
| Broadleaf Evergreen | |
| Acacia melanoxylon | Black Acacia |
| Ceratonia siliqua | Carob Tree |
| Myoporum laetum | Myoporum |
| Metrosideros excelsa | New Zealand Christmas Tree |
| Melaleuca leucadendra | Cajeput Tree |
| GENERAL LANDSCAPE TREES | |
| Deciduous | |
| Pistacia chinensis | Chinese Pistache |
| Gleditsia triacanthos "Moraine" | Honey Locust |
| Koelreuteria paniculata | Goldenrain Tree |
| Broadleaf Evergreen | |
| Agonis flexuosa | Willow Myrtle |
| Eucalyptus lehmannii | Lehmann Eucalyptus |
| Leptospermum laevigatum | Australian Tea Tree |
| Melaleuca nesophila | Pink Melaleuca |
| ACCENT TREES | |
| Deciduous | |
| Prunus blireiana | Flowering Plum |
| Prunus cerasifera | Cherry Plum |
| Populus nigra "Italica" | Lombardy Poplar |
| Broadleaf Evergreen | |
| Olea europaea | Olive |
| Acacia baileyana | Bailey Acacia |
| SCREEN OR BACKGROUND PLANTING | |
| Deciduous | |
| Ulmus parvifolia | Chinese Elm |
| Ulmus pumila | Siberian Elm |
| Broadleaf Evergreen | |
| Cupressocyparis leylandii | Leyland cypress |
| Eucalyptus camaldulensis | Red Gum |
| Eucalyptus citriodora | Lemon Scented Gum |
| Eucalyptus lehmannii | Bushy Yate |
| Eucalyptus polyanthemos | Silver Dollar Gum |
| Eucalyptus sideroxylon | Red Ironbark |

| <u>BOTANICAL NAME</u> | <u>COMMON NAME</u> |
|---------------------------------------|---------------------------|
| Coniferous | |
| Pinus muricata | Bishop Pine |
| Pinus eldarica | Mondell Pine |
| Pinus halepensis | Aleppo Pine |
| Pinus radiata | Monterey Pine |
| SHRUBS/HEDGES | |
| Buxus microphylla (japonica) | Japanese Boxwood |
| Carissa grandiflora | Natal Plum |
| Ceanothus gloriosus | Point Reyes Creeper |
| Cistus purpureus | Orchid Rockrose |
| Cotoneaster lactea | Cotoneaster |
| Dodonaea viscosa | Hopseed Bush |
| Escallonia rubra | Escallonia |
| Euryops pectinatus | Euryops |
| Grevillea "Noellii" | Grevillea |
| Griselinia littoralis | Griselinia |
| Hakea suaveolens | Sweet Hakea |
| Hebe "Patty's Purple" | Purple Veronica |
| Nerium oleander | Oleander |
| Pittosporum crassifolium | Pittosporum |
| Pittosporum tobira | Tobira |
| Raphiolepis indica | India Hawthorn |
| Xylosma congestum | Xylosma |
| GROUNDCOVER | |
| Arctotheca calendula | Cape Weed |
| Baccharis pilularis | Dwarf Coyote Brush |
| Coprosma kirkii | Creeping Coprosma |
| Cotoneaster dammeri | Bearberry Cotoneaster |
| Drosanthemum floribundum | Ice Plant |
| Hedera helix | English Ivy |
| Hypericum calycinum | St. Johnswort |
| Lampranthus species | Ice Plant |
| Vinca minor | Dwarf Periwinkle |
| VINES | |
| Bougainvillea | Bougainvillea |
| Jasminum mesnyi | Primrose Jasmine |
| Lonicera hildebrandiana | Giant Burmese Honeysuckle |
| Tecomaria capensis | Cape Honeysuckle |
| SPECIAL FLOWERING GROUNDCOVERS | |
| Agapanthus africanus | Lily-Of-The-Nile |
| Gazania species | Gazania |
| Osteospermum fruticosum | Trailing African Daisy |
| Trachelospermum jasminoides | Star Jasmine |
| Potentilla verna | Spring Cinquefoil |

Paving

Careful consideration of paving color, pattern, and texture should be given in order to tie together the "urban floor" of Town Center. Paved areas along the Central Promenade could serve as the origin of a special unifying linear paving pattern which branches out throughout the site. Within this continuous element, special patterns and materials can be used to provide identity, character and scale to different use areas such as plazas, seating areas, pedestrian corridors and building entries.

Major intersections tie into the unifying paving elements and display a special pattern defining the crosswalk. The design of these intersections varies to reflect a hierarchy of importance, yet relates to the overall family of forms and patterns.

Ideally, paving materials harmonize with building materials and are related in pattern and color throughout the site. The unifying paving element should be contrasting bands of materials and focused in the Central Promenade area. This unifying paving element ties the plaza areas and other special interest areas together throughout the rest of the Town Center site.

Flexible paving is recommended to accommodate the total and differential settlements expected on the site. Preferred paving materials include any form of modular or unit paving which can be re-set, or any materials such as asphalt which can withstand the differential settlements. The range of materials includes granite, cobble, brick, concrete and asphaltic concrete pavers, etc.

"Turf block" or an equal type paver is best used in landscaped areas where the need exists for occasional access, as for fire or emergency. The use of this type of material allows access while maintaining the appearance of the landscaped area.

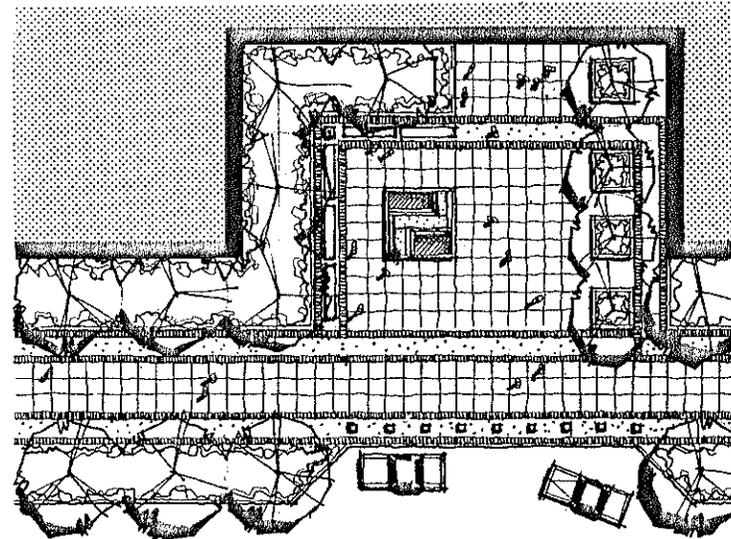
Site Furnishings

Site furnishings include benches and seating elements, walls and fences, planters and pots, waste receptacles, bollards, tree grates and guards, fountains and lighting.

Schematically, these furnishings form a coordinated, unified family of elements which give scale and character to open spaces within the site. Preferably, furnishings relate to a unified design theme carried throughout Town Center, and are designed for durability, appearance, comfort and safety.

Furnishings are most likely concentrated in the Central Promenade areas, building plazas, and other pedestrian activity areas. These elements are clustered into functional groupings and organized relative to paving and building modules.

Site furnishings should be related in terms of color, texture, form and detailing. Any metal parts or furnishings such as flagpoles, light fixtures, fences, trash receptacle openings, etc., should be of a consistent color and finish. Wood may be combined with concrete to add comfort and character to benches and seating.



Prototypical Site Furniture Clustering

Benches and Seating Elements should be provided in appropriate areas within the development. Seating areas will be the most successful in sheltered positions overlooking active gathering and pedestrian circulation areas. It is best to offer a variety of seating options, including those in sun or shade, in clustered groups, and in active as well as private, passive areas.

Walls and Fences may be used for visual privacy, separation, and screening purposes. Long lengths of wall should be visually broken by setting back portions, or providing alternating panels of materials. Visibility may be increased where desirable by using metal vertical members or fenestrations within the wall. Where walls adjoin buildings, the materials and design should relate to the structure.

Planters and Pots can be used to break up paved areas and provide textural and color interest on a more intimate scale. In addition, movable pots allow for flexible plaza space.

Waste Receptacles need to be concentrated in high use areas such as plazas, eating areas and building entries. Maintenance is accommodated by providing drainage holes and removable liners.

Bollards may be used to prevent cars from being driven or parked in pedestrian spaces and may contain pedestrian-scale lighting. Removable bollards are recommended to facilitate emergency or special access.

Tree Grates and Guards enhance paved areas of concentrated pedestrian activity, such as the Central Promenade and building plazas. Cast iron grates and guards are durable and will protect the soil roots and trunks of trees and add a special urban character to the landscape.

Special Elements such as fountains, sculpture, banners, and flags are encouraged in Town Center to add vitality and uniqueness to the public spaces. These elements have the greatest visual impact when located at their highest use potential and when concentrated in the central pedestrian promenade and central open space. Linear or intersecting visual corridors, building plazas, and pedestrian niches provide an opportunity for effective focal elements.

Water, in an urban context of sculpture and fountains, is encouraged as a theme for Town Center and also best utilized in the Central Promenade and central open space area. Water features may also be associated with building plazas and other outdoor pedestrian gathering areas, but should not overpower or compete with the central core features. Due to the emphasis on water conservation, as well as problems of proper maintenance, water features should be designed to be sculptural and visually attractive with or without water. The use of water should be kept simple and low, with no high or fine sprays that could be scattered by the winds experienced on the site.

Lighting

Site lighting has a significant effect on the character and safety of outdoor vehicular and pedestrian use at night. Lighting is designed to reinforce the hierarchy of vehicular streets as well as pedestrian spaces and reinforce identity and unity along these corridors. Other areas that deserve special lighting considerations include parking areas, the Central Promenade and central open space, building entries and plazas,

project entries, residential area entries, and signs. Opportunities for seasonal or event lighting can also be provided, especially in the core activity areas.

Lighting of major streets will conform to Foster City standards and will be spaced for maximum effectiveness in the central medians.

Parking area lighting will be of a consistent height throughout Town Center. Lighting design should be sensitively executed to prevent glare and light spillover. Lighting associated with pedestrian areas should be at a lower height and scale and of a color which maximizes natural color schemes.

Using lighting fixtures related in design and materials will result in compatibility with the architecture and site furnishings. The shape of the fixtures should be consistent and exhibit clean, simple lines.

Irrigation

Water conservation is a primary consideration in the irrigation design at Town Center. Low level conventional spray systems may be used for turf areas. Drip emitters are recommended for groundcover and shrub beds and trees in grates where feasible.

The watering program should also be short and frequent versus long and at greater intervals to promote shallow root growth. This strategy will also help to mitigate the constraints of the high groundwater table.

Erosion Control

Measures must be taken in order to control erosion and runoff before, during, and after construction. Temporary controls may include hydroseeding before and after construction phasing. This operation will reduce the loss of topsoil and nutrients from the site. The initial perimeter plantings by the Assessment District will serve as a vegetative windbreak, thereby reducing wind erosion.

Erosion and sedimentation need to be controlled during construction phases. Retention ponds can be constructed in the undeveloped areas of the site to be used to minimize the sediment loading of the City drainage system. In addition, all cleared areas subject to erosion can be hydro-seeded to provide cover for erosion control.

The contractor will be required to control dust at the work site during construction operations by the use of an adequate number of water trucks or an effective palliative. Dust-laying equipment and other necessary

methods can be employed to protect public and private property in the vicinity or adjacent to the area from annoyance or damage from dust. The contractor will be required to conduct dust-control operations at all times during construction.

Prototypical Treatments

The following prototypical design solutions presented in this section are meant to be used as general guides for typical landscape situations occurring in Town Center. These solutions are taken from specific areas on the Section Key Plan, and serve as prototypes which can be adapted or modified during the specific use process to suit the varying site conditions.

Perimeter Landscape Treatments

The landscape of perimeter areas of the General Plan is described in the following discussion. These include the buffer along State Route 92 and the landscape treatments along Foster City, East Hillsdale and Edgewater Boulevards.

Edgewater Boulevard

The prototypical treatment for Edgewater Boulevard is a continuation of the treatment proposed along Town Center Boulevard (Key 4, Site Landscape Concept). A minimum 21.5' landscape easement and right-of-way includes an 8' bike path and a 5' sidewalk separated

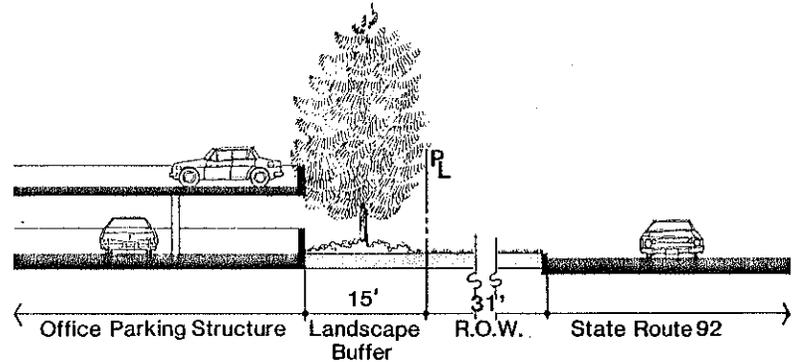
East Hillsdale Boulevard

East Hillsdale Boulevard links the Town Center core and the existing Leo J. Ryan Park (Key 2, Site Landscape Concept). At the Town Center edge, the prototypical treatment calls for a 40' landscape buffer with a double row of broadleaf evergreen street trees and evergreen hedge plants screening the parking areas

while providing a green edge to Leo J. Ryan Park. Along this corridor is an 8' bikepath and a 5' sidewalk, separated from vehicular traffic and from each other by the tree plantings.

State Route 92 Buffer

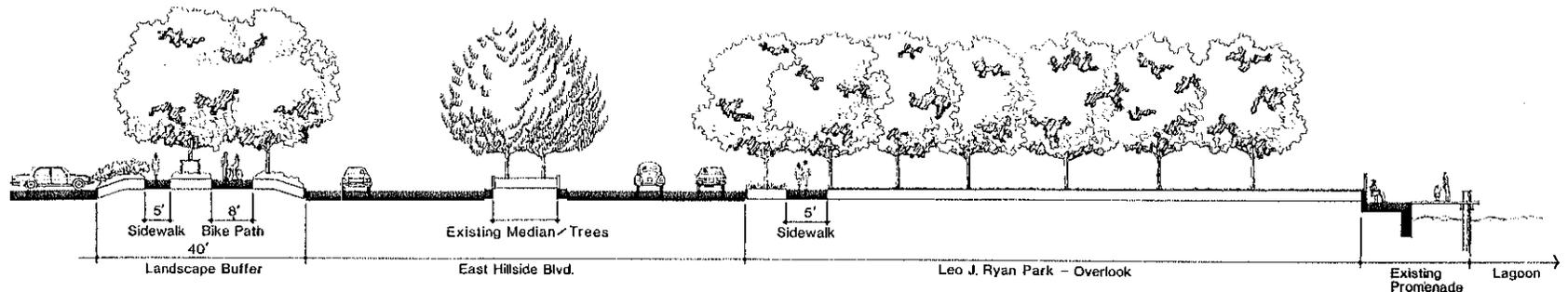
Two parking structures and several parking areas are adjacent to State Route 92 along the northern site boundary. It is proposed at this edge to plant coniferous evergreen trees in a tightly spaced row within a landscaped buffer consisting of private lands and lands controlled by CalTrans. Where the proposed future overpass enters the site on the northern boundary, views will open up into the central axis of the site.



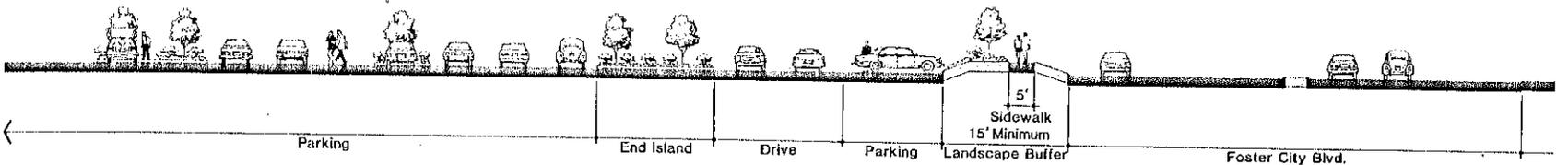
Section D - State Route 92 - Buffer

Foster City Boulevard

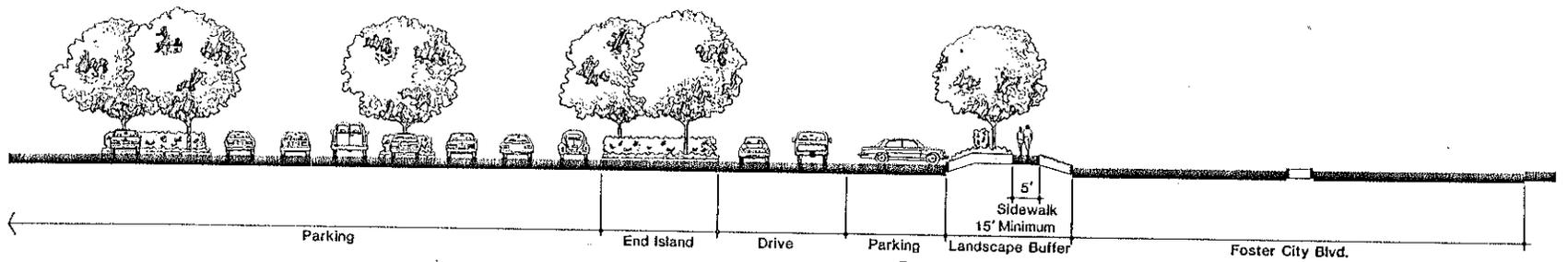
The regional retail building and surface parking lot borders Foster City Boulevard to the east. (Key 6, Site Landscape Concept). Here a minimum 15' wide landscape buffer is proposed with formal broadleaf evergreen street trees. A dense evergreen hedge will serve to further screen views of the parking lot.



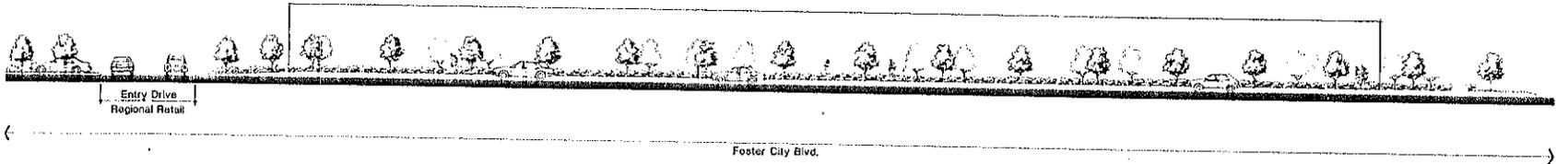
Section E - East Hillsdale Blvd.



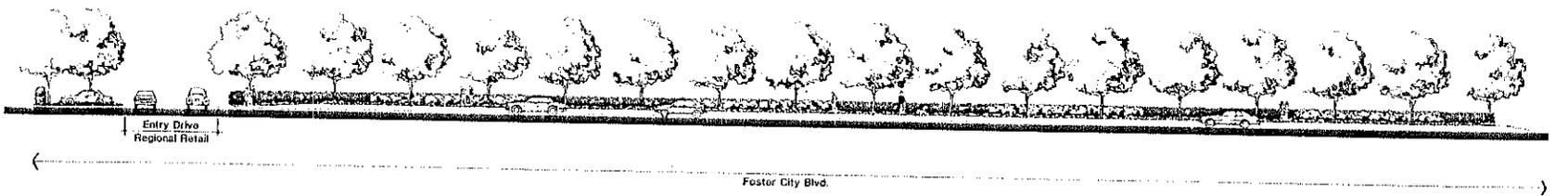
Section F - Initial Planting Foster City Blvd. at Regional Retail



Section F - Mature Planting Foster City Blvd. at Regional Retail



Section G - Initial Planting Foster City Blvd. at Regional Retail



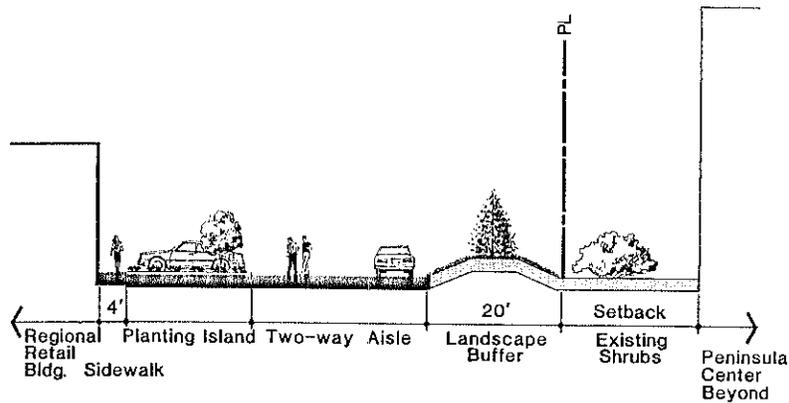
Section G - Mature Planting Foster City Blvd. at Regional Retail

Landscape Treatments at Differing Uses

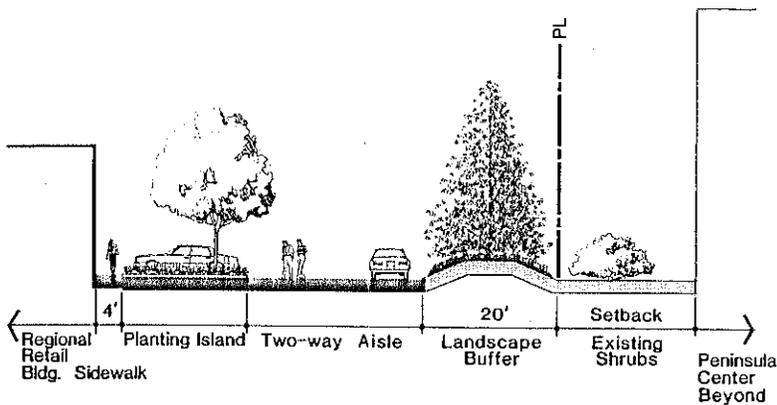
Special attention has been given to the landscape areas between differing uses on the General Plan. This includes the regional retail site where it interfaces with the adjoining off-site office, and the townhouse residential where it interfaces with offices.

Regional Retail - Office

The regional retail parcel is adjacent to existing off-site mid-rise office structures. The prototypical landscape treatment for this buffer is a 20' landscaped area with tightly spaced coniferous evergreen trees, completing a dense visual screen. In addition, tree planting islands with large deciduous trees are proposed along the south side of the regional retail building in order to enhance this visual buffer.



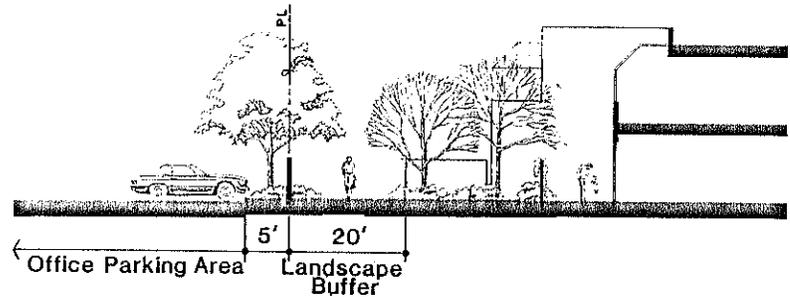
Section H - Initial Planting Regional Retail Buffer



Section H - Mature Planting Regional Retail Buffer

Townhouse Residential - Office

Where the two townhouse residential parcels adjoin office parcels to the north and south, a combination vegetative screen and wall or fence is proposed as the prototypical buffer treatment. A wall or open metal fence is indicated on the property line, and is to be used with a groundcover and broadleaf evergreen trees along the 5' landscape strip within the office parcel.



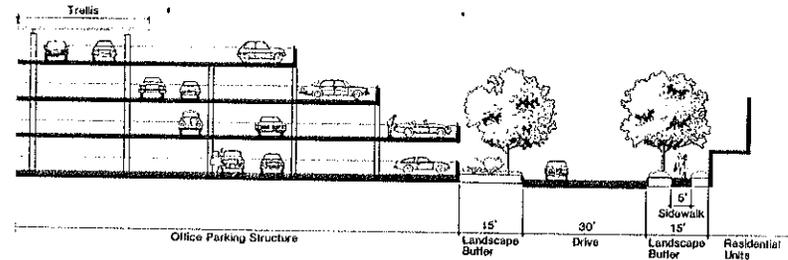
Section I - Townhouse Residential at Office

Roadway Treatments

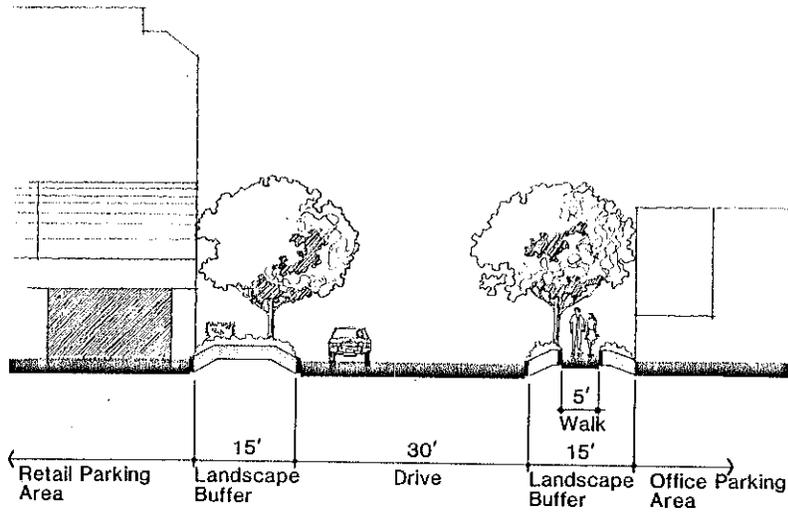
Typical sections have been developed for roads within and adjacent to the Town Center site. The various treatments begin to establish a hierarchy of roads and landscape treatments, as well as indicating walks and bikeways.

Internal Drives

The internal circulation drive surrounding the mid-rise core area is treated with formal rows of broad-leaf evergreen street trees (Key 8, Site Landscape Concept). A 5' sidewalk follows the outer edge of the drive and serves office and residential foot traffic. Where the drive borders retail and office parking areas, these 15' landscape buffers are planted with groundcover and hedges for additional screening.



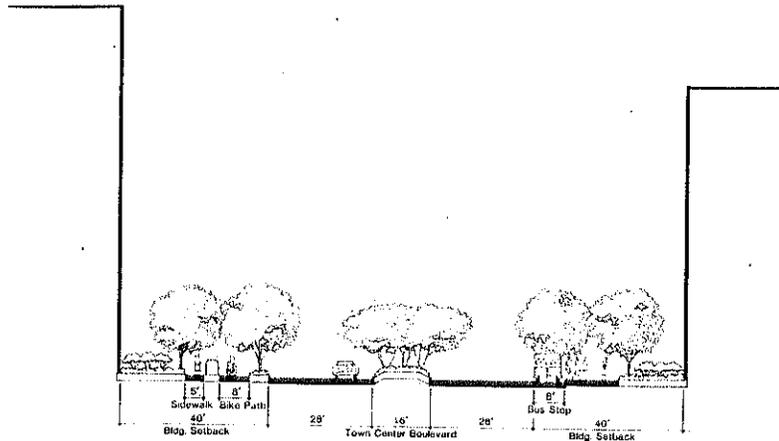
Section J - Internal Drive at Structures



Section K - Internal Drive at Parking Areas

Town Center Boulevard

Town Center Boulevard is defined as the major circulation corridor, using double rows of tall broadleaf evergreen trees on each side accented by small deciduous flowering trees in the median (Key 3, Site Landscape Concept). Typically, a 5' sidewalk and 8' bikepath follow along the south side of the street and are separated by a hedge.



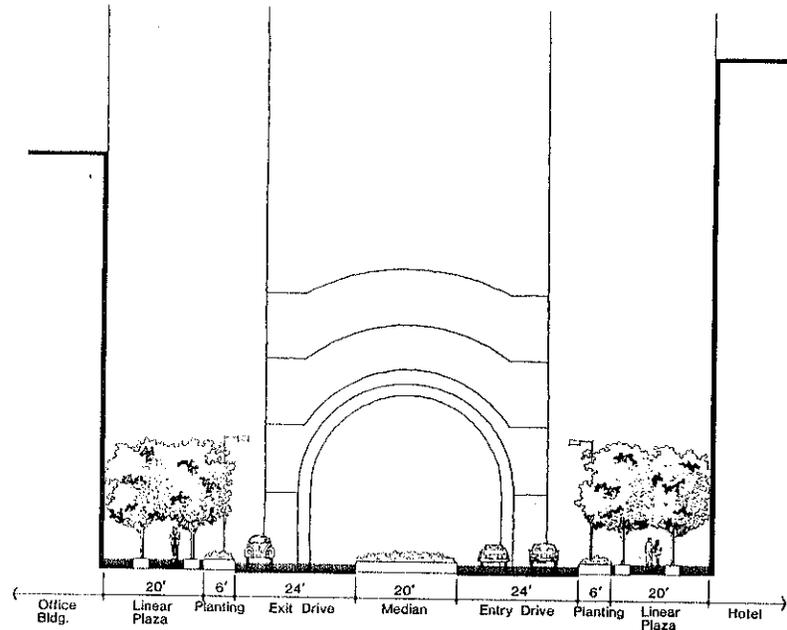
Section L - Town Center Blvd.

Shell Boulevard

Shell Boulevard maintains a cross-section similar to Town Center Boulevard, with rows of broadleaf evergreen trees on each side and a sidewalk and bikepath continuing down the west side of the street. (Key 5, Site Landscape Concept).

North Entry Drive

The north entry drive from Town Center Boulevard signals the beginning of the urban character of the central core of Town Center. This typical cross section has a double row of small broadleaf evergreen trees on each side of the drive. A 20' wide central median of flowering groundcover and similar planting strips on each side create a strong band of color visually leading toward the central core and landscaped open space.

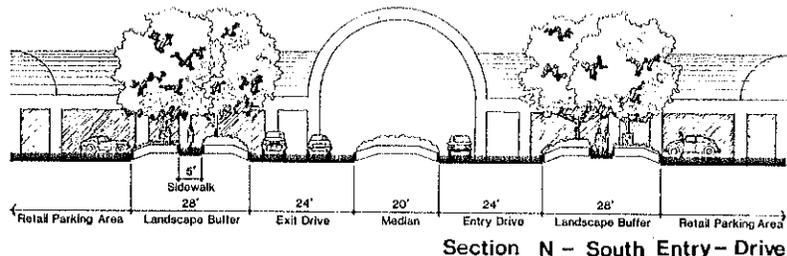


Section M - North Entry Drive

South Entry Drive

The south entry drive off East Hillside Boulevard is reinforced by double rows of tall broadleaf evergreen accent trees (Key 1, Site Landscape Concept). Like the north entry drive, a 20' flowering median and similar planting strips along the drive enhance the

visual connection into the central core and landscaped open space. This drive is visually separated from the retail parking areas by 28' landscape buffers with ever-green hedges and plantings. A 5' sidewalk connects East Hillsdale Boulevard to the core through these landscape buffers on each side.

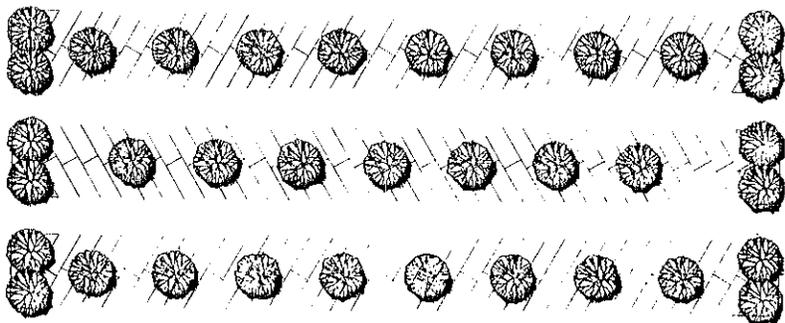


Surface Parking Area Treatments

Four prototypical landscape treatments are proposed for surface parking areas of Town Center. These treatments include two approaches for retail parcel parking areas, and two approaches for office parcel parking areas. The retail parcels differ from the offices in that they utilize angled versus ninety degree parking stalls.

Retail Parcels with Tree Wells

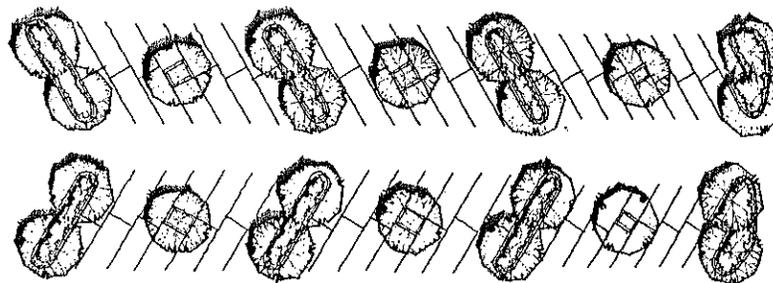
In these parking areas, angled parking works with individual tree wells on ± 40 foot spacings. Compact car spaces provide the extra space needed to create the wells in the center. Trees and river rock or groundcover serve as plantings within the wells.



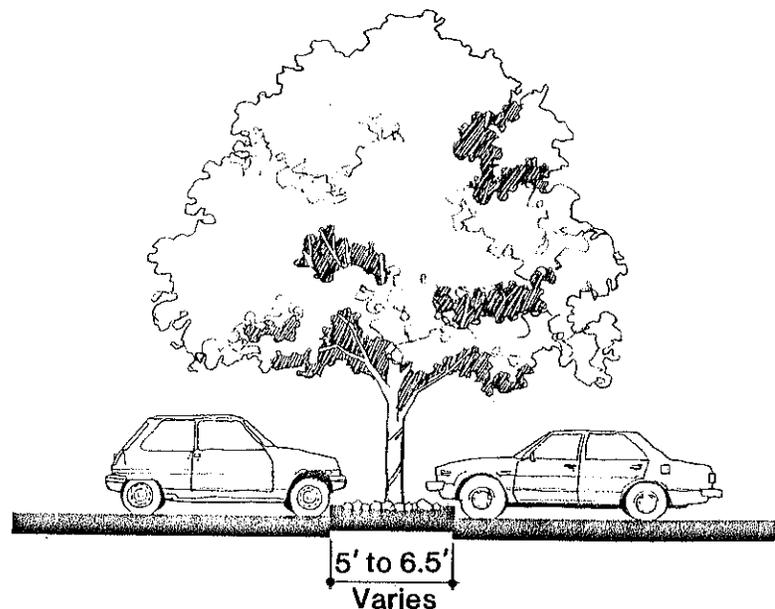
Tree Wells - Retail Parking Area

Regional Retail Parcel

In the parking area for the regional retail site, tree wells alternate with planting islands. The planting islands extend the width of a full-size parking stall and should be treated with trees and hedges.



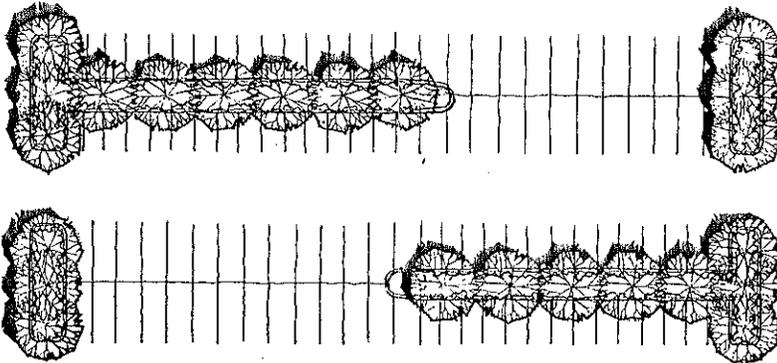
Tree Wells - Regional Retail Parking Areas



Typical Section - Tree Well

Office Parcels With Staggered Medians

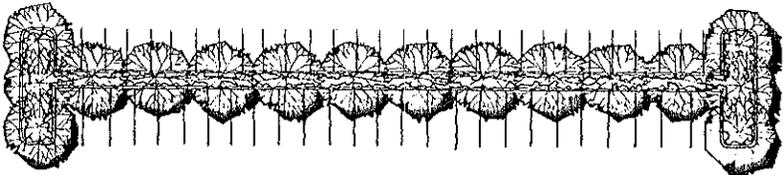
When appropriate, staggered median treatment will be utilized in the office parcel parking areas. This is a median which runs in alternating spans down each parking bay, alternating positions with the next median. The median portion is flanked on either edge by compact car stalls, and where the median stops, two full size car stalls begin. As in all medians, formal rows of trees are used to screen the parking area and breakdown the visual scale.



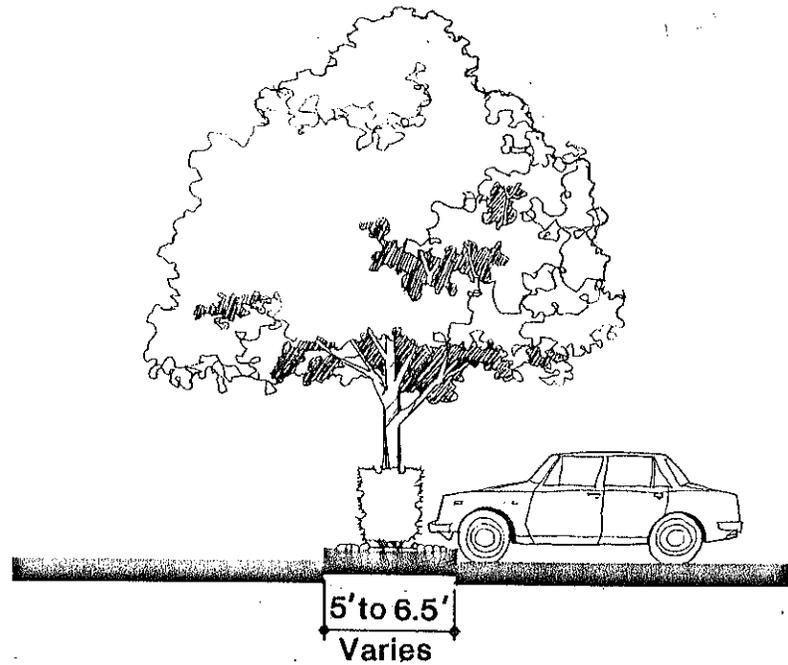
Alternating Office Parking Areas

Office Parcels with Continuous Medians

Where appropriate, landscaped medians running the entire length of the parking bays will be utilized in the office parcel parking areas. Formal rows of trees form visual buffers within these median areas.



Continuous Office Parking Areas



Typical Section - Median

Parking Guidelines

Ratios of the number of parking stalls for unit areas of different building types, ratios of the number of compact to full-size car parking stalls, and recommended parking stall dimensions were developed for off-street parking at Town Center. Table 1 presents the recommended parking ratios to be provided for each land use at Town Center. A compact car ratio of 40% is recommended for office and residential uses and 30% for retail and restaurant uses. Recommended parking stall dimensions for different types of parking areas at Town Center are presented in Table 2.

Table 1

| RECOMMENDED PARKING RATIOS FOR TOWN CENTER ^a | | |
|------------------------------------------------------------|--------------------------------------------------|------------------------------------------------------|
| USE | RECOMMENDED PARKING RATIO | CURRENT FOSTER CITY PARKING RATIO ^b |
| Office | 3.3 stalls/1000 sq. ft. floorspace | 5.0 |
| Retail | 4.0 stalls/1000 sq. ft. floorspace (GLA) | 5.0 |
| Restaurant | | |
| Fast-Food | 15.0 stalls/1000 sq. ft. floorspace ^c | 40.0 |
| Sit-Down | 12.5 stalls/1000 sq. ft. floorspace ^c | 40.0 |
| Residential | | |
| Town Houses and Mid-rise units | | |
| 3 - Bedroom units | 2.5 stalls/unit ^d | 4.00 ^e |
| 2 - Bedroom units | 2.0 stalls/unit ^d | 3.25 ^e |
| 1 - Bedroom units | 1.5 stalls/unit ^d | 2.50 ^e |
| Studios | 1.2 stalls/unit ^d | 2.50 ^e |
| Hotel | 1.0 stall/room | 1.0 stall/room |

- The central landscaped open space perimeter retail, which may include small eateries and shops, is primarily support services oriented to meet the needs of the surrounding office, residential, and hotel users. Therefore, the parking requirement for this retail element is minimal, and the parking ratios reflected herein are not applicable to this element.
- Foster City Zoning Ordinance Chapter 17.62.
- These ratios would be reduced by 2.5 stalls/1000 sq. ft. if a shared parking plan were developed for the restaurant and nearby office uses.
- The recommended parking ratios for town homes & condominiums include 0.2 stalls/unit for visitors.
- Current Foster City parking ratios include 1.0 stall/unit for visitors.

SOURCE: Wilsey & Ham, October 1983 Revised 11/7/83

Table 2
RECOMMENDED AND CURRENT STANDARD
FOSTER CITY STALL DIMENSIONS

| PARKING AREA TYPE (USE) | STALL SIZE AND CONFIGURATION | RECOMMENDED SIZE (FT) ^a | LEVEL OF CONVENIENCE ^b | FOSTER CITY STANDARD (FT) ^c |
|-----------------------------------------------|------------------------------------|------------------------------------------|--------------------------------------|----------------------------------------------|
| Low Turnover Surface (Office) | Regular - 90° | 8.5 X 18.5 | B | 8.5 X 20 |
| | Compact - 90° | 7.5 X 15 | B | 8.0 X 17 |
| High Turnover Surface (Retail, Restaurant) | Regular - 90° | 9.0 X 18.5 | A | 9.0 X 20 |
| | Compact - 90° | 8.0 X 15 | A | 8.0 X 17 |
| | Regular - 60° | 10.4 X 19.0 | A | 10.4 X 20 |
| | Compact - 60° | 9.2 X 15.5 | A | 9.2 X 17 |
| Structured (Office, Residential) | Regular - 90° | (8.5 to 10.0) X 18.5 ^a | A | 9.0 X 20 |
| | Compact - 90° | (7.5 to 8.3) X 15 ^a | A-B | 8.0 X 17 |

- Dimensions indicated refer to stall width (measured parallel to aisle) and stall depth (measured perpendicular to aisle). Additional widths will be provided for all column and end stalls. Handicapped parking stalls will be in conformance with current State Architect's Regulations.
- These levels of convenience have been established in the Functional Design of Parking Structures (Ref. 13). Levels A and B represent a door-opening clearance more than adequate to meet the needs of average short-term parking operations (5 or more changes per day). Level C is adequate for long-term parking operations (1 or 2 changes per day). Levels D and E are only satisfactory for attendant parking.
- Foster City (Recommended) Parking Standard, January 1981 (Ref. 12). Dimensions indicated refer to stall width (measured parallel to aisle) and stall depth (measured perpendicular to aisle).

SOURCE: Wilsey & Ham, October 1983

Signage Guidelines

An effective signage and graphics system functions as an integral part of its environment. Unified and controlled signage mutually benefits all tenants, residents and visitors in projecting information in a cohesive and consistent theme.

To maintain the consistency of the architectural image of Town Center, no signs shall be permitted within the project boundaries that are not part of, or do not conform to the system. The continuity of this architectural image is created through the use of a minimal application signage system. The creation of this system satisfies all essential signage requirements while preventing the confusion created by utilization of diverse systems. In addition, it is recognized that all commercial users will need vehicular identity. The implementation of this system will reinforce the quality business and residential environment developed by Town Center.

Included in this section are Signage Objectives and Signage Review and Approval.

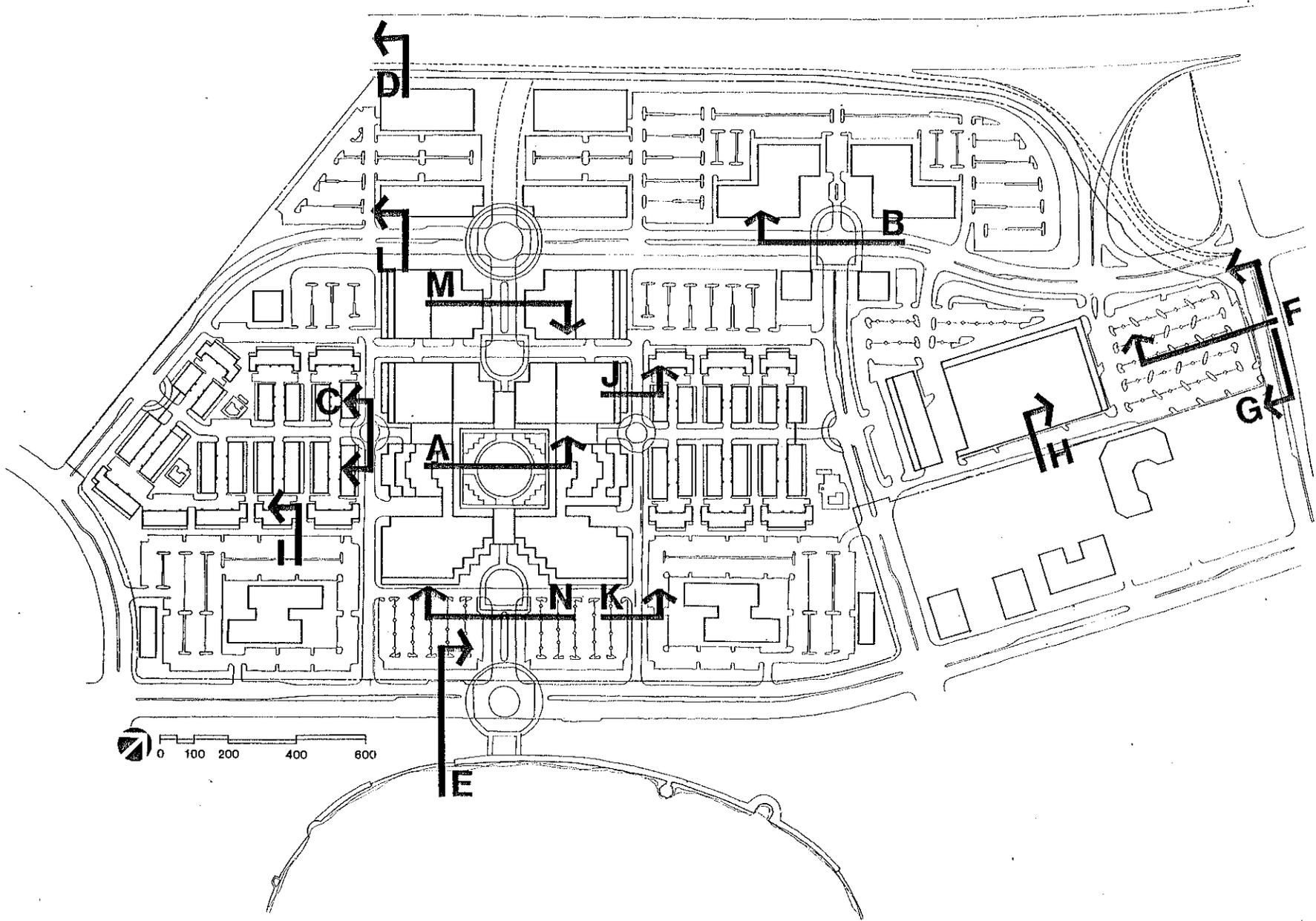
Signage Objectives

- To allow sufficient, though not excessive, business and residential identification devices so that the tenants and residents in Town Center are clearly associated with the facilities they occupy, when viewed by motorists or pedestrians passing by on fronting streets and walkways
- to control signs and business identification devices which are used for advertising of any kind, including advertising of products, services or job openings
- to allow sufficient, though not excessive, informational, directional, and traffic control signing for convenient, efficient and safe operation
- to have all informational, vehicular control, and temporary signing to be uniform in design, size, height, color, material, and typography
- to permit sufficient, though not excessive, interim signing, including those identifying the developer, future occupants, design consultants, general contractor, lending institutions, and marketing information.
- to insure that signing and identification devices do not contribute to a decrease in the safety and efficiency of traffic flows in fronting streets
- to insure that signing and identification devices adhere to the zoning regulations as determined by the City of Foster City
- to establish a signing and identification system which is highly legible, durable, vandal-resistant, and complements the architectural concept of the development.
- to allow for graphic, colored fabrics that complement surrounding architectural details
- to allow, in appropriate spaces, for the use of banners and flags to promote an open, festive feeling by adding color and motion.

Signage Review and Approval

To assist individual occupants in producing On-Building and Off-Building Signage which complements the archi-

tectural concept of Town Center, an architectural review committee will be established. Project developers, architects, and graphic designers will form this review and approval board to insure conformity to established guidelines. The committee will offer examples of designers, fabricators and contractors to promote a uniform, high level of quality in all phases of design.



Section Key Plan